



**BROOKLYN 1B**  
1309 NE 66TH ST

DESIGN REVIEW RECOMMENDATION  
25 JUNE 2018  
DPD #3026788



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3.0 PROJECT PROPOSAL



PROJECT INFORMATION

ADDRESS: 1309 NE 66TH ST  
PROJECT #: 3026788

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- DEVELOPMENT OBJECTIVES**
- GROSS SQUARE FOOTAGE: **56,200 SF**
  - 7 LEVELS ABOVE GRADE
  - 77 UNITS
  - 38 PARKING SPACES BELOW GRADE
- PRIMARY DESIGN FEATURES**
- LEVEL 1 & 2 - 13'-9" SETBACK ON NE 66TH ST
  - LEVEL 1 & 2 - 9'-8" SETBACK ON BROOKLYN AVE NE
  - RESIDENT LOBBY AND AMENITY SPACES AND A ROOF DECK



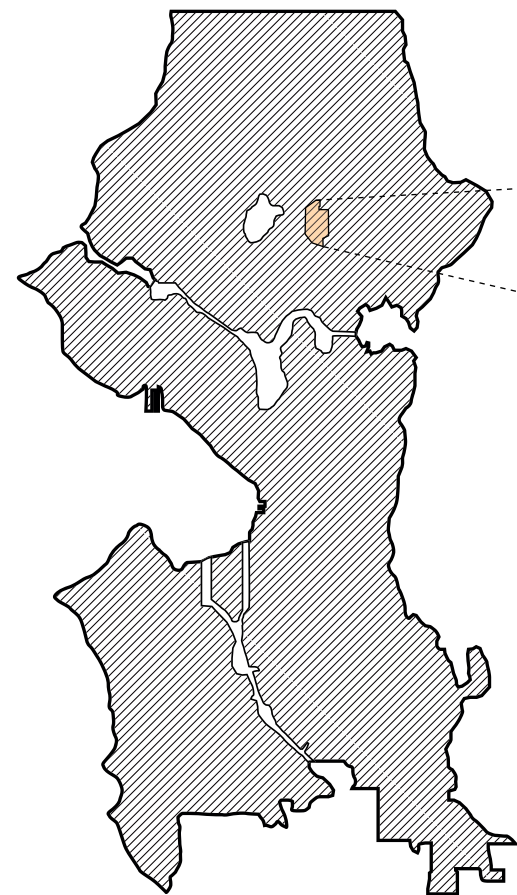
PROJECT GOALS

- PROVIDE A HIGH QUALITY LIVING ENVIRONMENT FOR RESIDENTS WITH ACCESS TO PUBLIC TRANSPORTATION, RETAIL, RESTAURANTS, AND OUTDOOR AMENITIES
- PROVIDE A PEDESTRIAN-ORIENTED STREETFRONT ON NE 66TH ST (WIDEN SIDEWALK AND PLANT STREET TRESS, APPROPRIATELY SCALED FACADE)
- PROVIDE A RESIDENT-ORIENTED STREETFRONT ON BROOKLYN AVE NE (RESIDENTIAL ENTRY TO PROVIDE EYES ON THE STREET, LANDSCAPE TO BLEND WITH NEIGHBORS, SAFE AND ATTRACTIVE PARKING ENTRY, LOCATE TRASH AND RECYCLING STORAGE OUT OF SIGHT)
- PROVIDE EYES ON THE STREET ON NE 66TH ST (INDIVIDUAL GROUND LEVEL PATIOS & UNIT DECKS)
- PROVIDE A WELL PROPORTIONED BASE
- FIT WITH SURROUNDING PROPOSED ADJACENT DEVELOPMENTS

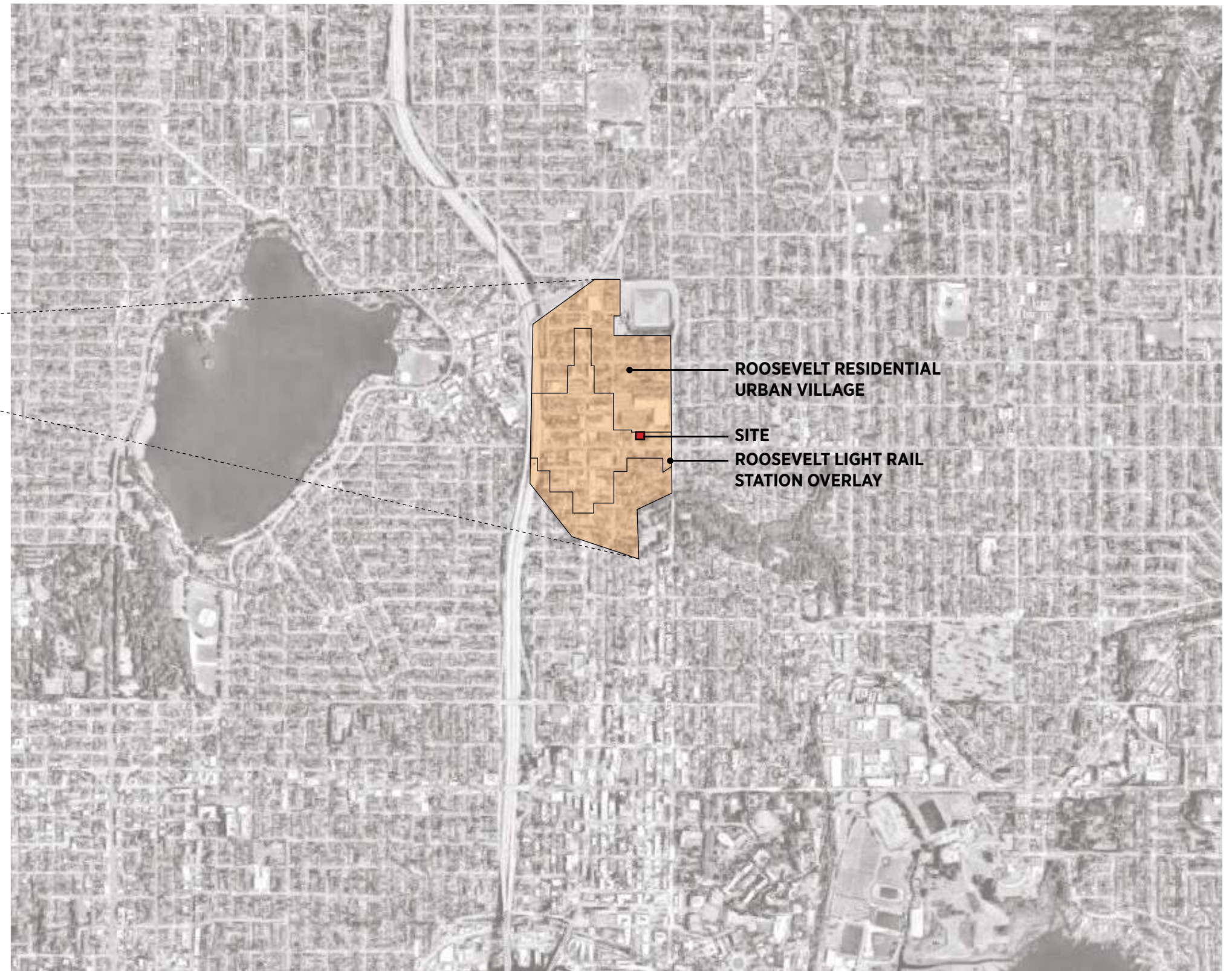


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VICINITY MAP





## ZONING





## LAND USE



### LEGEND

Future Light Rail Station (2021)	
Future Development	
Single Family Residential	
Multi Family Residential	
Mixed Use	
Retail	
Office	
Surface Parking	
School / Institutional	
Utility	
Parks/Open Space	
Roosevelt Commercial Core	



4.0 SUMMARY CONTEXT ANALYSIS

TRANSIT

The project site is well served by several bus lines located within a 5-minute walk radius. King County Metro bus routes run along NE 65th Street:

- 45 - Loyal Heights, UW Station
- 62 - Hawthorne Hills, Queen Anne, Downtown
- 64 - Jackson Park, UW
- 71 - Wedgewood, UW Station
- 76 - Wedgewood, Hawthorne Hills

The relative flatness of NE 66th Street provides an easy walk to the future light rail station and is slated to become a bicycle route and Green Street. Roosevelt's commercial core is within a 5-minute walk to the west as is Cowen Park. Roosevelt High School is across the street.

Travel times to the University, Capitol Hill and Downtown will greatly improve with the opening of Sound Transit light rail service to Roosevelt Station located just one block to the west.

LEGEND

Arterial

Future Light Rail Station (2021)

Gateway Intersection per Roosevelt

Frequent Transit Corridors

Pedestrian Overlay

5 Minute Pedestrian Walkshed

Bus Routes

Bus Lines

Current Bicycle Routes

Proposed Bicycle Routes per Seattle Bicycle Master Plan

Proposed Green Street per Roosevelt Streetscape Concept Plan

2





FUTURE NEIGHBORHOOD CONTEXT

LEGEND

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. The Westy Roosevelt
- 4. Whole Foods
- 5. Vacant
- 6. Silhouette Antiques and Gifts
- 7. Future Transit Oriented Development Sites
- 8. Cowen Park
- 9. Ravenna Park
- 10. Dwell Condomiums & Heartbeet Cafe
- 11. Roosevelt Square
- 12. Toronado
- 13. Health Mutt
- 14. Qwest Communications
- 15. Future 4-Story Mixed-Use Project
- 16. Future 5-Story Mixed-Use Project
- 17. Future 6-Story Mixed-Use Project
- 18. Future 7-Story Mixed-Use Project
- 19. 4-Story Mixed-Use Project
- 20. Intercommunity Peace & Justice Center
- 21. Elements Massage





EXISTING NEIGHBORHOOD CONTEXT

LEGEND

- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. The Westy Roosevelt
- 4. Whole Foods
- 5. Vacant
- 6. Silhouette Antiques and Gifts
- 10. Dwell Condomiums & Heartbeet Cafe
- 11. Roosevelt Square
- 12. Toronado
- 13. Health Mutt
- 14. Qwest Communications
- 18. 4-Story Mixed-Use Project
- 20. Intercommunity Peace & Justice Center
- 21. Elements Massage
- 22. Mixed Use Building
- 23. Site Cleared
- 24. Single Family Houses
- 25. Under Construction & Staging



AXONOMETRIC VIEW



## FUTURE NEIGHBORHOOD CONTEXT



AXONOMETRIC VIEW



### LEGEND

- Future Link Light Rail Station
- 1. Roosevelt High School
- 2. Rising Sun Produce
- 3. The Westy Roosevelt
- 4. Whole Foods
- 5. Vacant
- 6. Silhouette Antiques and Gifts
- 7. Future Transit Oriented Development Sites
- 10. Dwell Condomiums & Heartbeet Cafe
- 11. Roosevelt Square
- 12. Toronado
- 13. Health Mutt
- 14. Qwest Communications
- 15. Future 4-Story Mixed-Use Project
- 16. Future 5-Story Mixed-Use Project
- 17. Future 6-Story Mixed-Use Project
- 18. Future 7-Story Mixed-Use Project
- 19. 4-Story Mixed-Use Project
- 20. Intercommunity Peace & Justice Center
- 21. Elements Massage





VIEW FROM ROOSEVELT HIGH SCHOOL





VIEW FROM ROOSEVELT ATHLETIC FIELDS



5.0 EXISTING SITE CONDITIONS



LEGEND

1. Roosevelt High School

2. Rising Sun Produce

3. The Westy Roosevelt

4. Whole Foods

5-9. Vacant

10. Silhouette Antiques and Gifts

11. Cowen Park

12. Ravenna Park

13. Dwell Condominiums & Heartbeet Cafe

14. Roosevelt Square

15. Toronado

16. Health Mutt

17. Qwest Communications

18. 4-Story Mixed-Use Project

19. Intercommunity Peace & Justice Center

20. Elements Massage

21. Mixed Use Building

22. Site Cleared

23. Sites Cleared

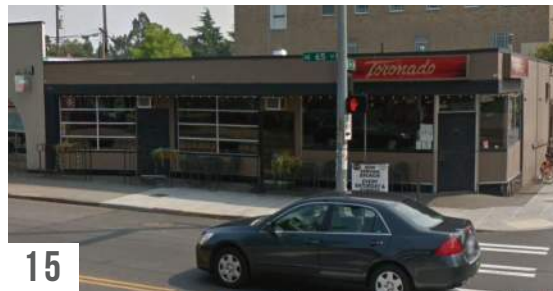
24-26. Single Family Houses

27-29. Under Construction & Staging

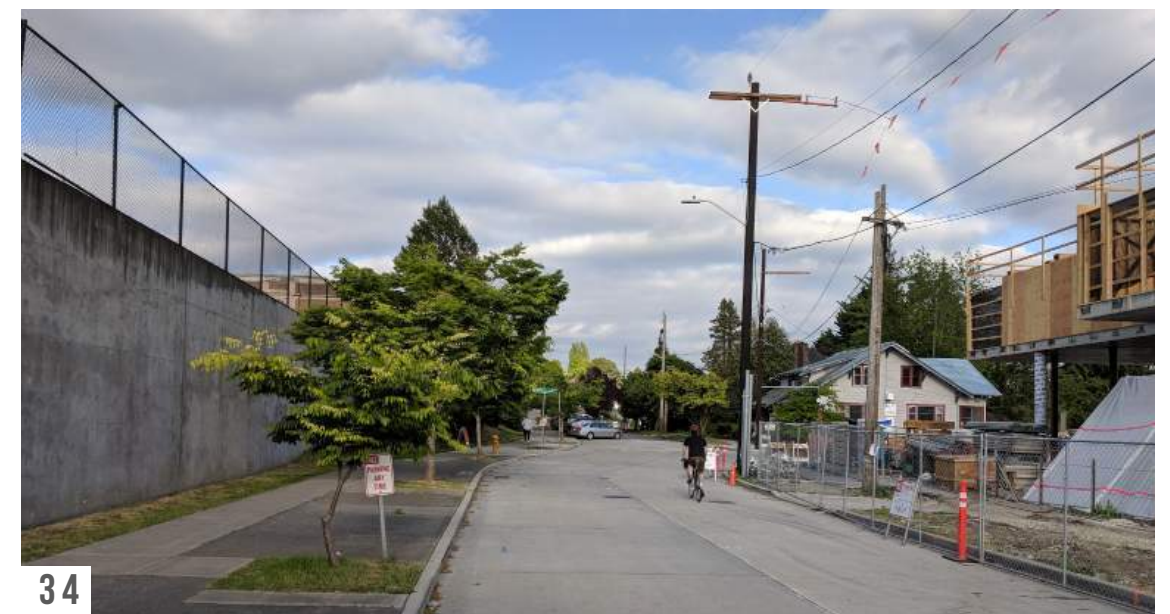
30. View Street from Site

31. View of Site from Roosevelt Athletic Fields

32-34. Views of Site at Street Level





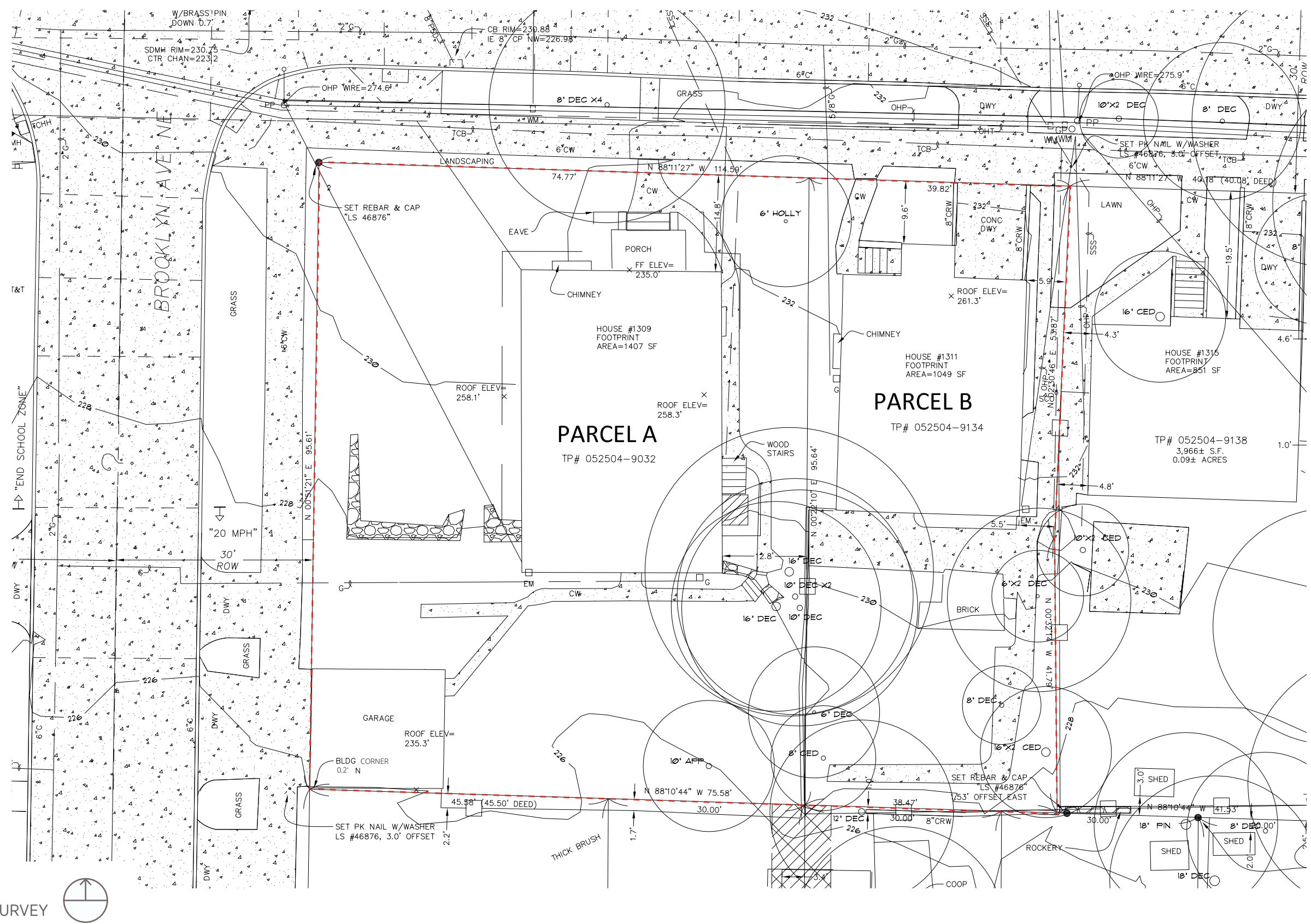




CODE SECTION		RESPONSE
<b>23.47A.008 STREET-LEVEL DEVELOPMENT STANDARDS</b>	<p>Section A: Basic Street-Level Requirements</p> <p>2.a Facades are blank if they do not include windows, entryways/doorways, stairs/stoops/porticos, decks/balconies, or screening/landscaping on the facade</p> <p>2.b Blank segments of street-facing facade between 2-8 ft above sidewalk may not exceed 20 ft in width</p> <p>2.c Total of all blank facade segments may not exceed 40% of the facade width along the street</p> <p>3. Street-level street-facing facades shall be located within 10 ft of the street lot line unless wider sidewalk or approved landscaping is provided</p> <p>Section D: Residential Uses On Street-Level Street-Facing Facade Requirements</p> <p>1. At least 1 of the street-level street-facing facades with residential uses shall have a visually prominent pedestrian entry</p> <p>2. Dwelling units shall be at least 4 ft above or below sidewalk grade or setback at least 10 ft from sidewalk</p>	<p>Blank segments of street-facing facades are less than 20 ft in width and less than 40% of facade in total.</p> <p><b>Departure is requested.</b> See section 17.0 Departures for additional information.</p> <p>Dwelling units are at least 10 ft setback from the sidewalk.</p>
<b>23.47A.009 STANDARDS APPLICABLE TO SPECIFIC AREAS</b>	<p>Section D: Roosevelt Urban Village</p> <p>1.a Setback requirements</p> <p>1) NE 66th St: an average ground level setback of 10 ft along the length of the street property line and a min upper level setback of 4 ft in addition at 45 ft above average finished grade</p> <p>2) Brooklyn Ave NE: an average ground level setback of 5 ft along the length of the street property line and a min upper level setback of 4 ft in addition at 45 ft above average finished grade</p> <p>1.b Structures in required setbacks subject to 23.47A.014.e except</p> <p>1) Decks with open railings may project up to 5 ft into required setback if they are 20 ft or more above grade. May not cover more than 20% of total setback area</p> <p>2) Stoops or porches with direct access to individual housing units may project 5 ft into the required ground level setback area. May not cover more than 20% of total setback area</p> <p>3) Fences no greater than 4 ft are permitted</p> <p>2. Ground level setbacks to be landscaped, may include paving</p> <p>3. Commercial uses prohibited within 80 ft of NE 66th St except within 50 ft of Brooklyn Ave</p> <p>4. Housing units at ground floor facing NE 66th St must have primary entrance directly accessible from exterior</p> <p>5. Parking to be located below grade; may extend up to 4 ft above existing or finished grade if screened or landscaped</p>	<p><b>Departure is requested.</b> See section 17.0 Departures for additional information.</p> <p>Housing units are provided with entrances directly accessible from the exterior.</p>
<b>23.47A.012 STRUCTURE HEIGHT</b>	<p>Section A: Maximum height per zone is 65 ft</p> <p>Section C: Rooftop features</p> <p>2. Open railings, clearstories, parapets and firewalls may extend up to 4 ft above the applicable height. Insulation, decks and soil for landscaping may extend up to 2 ft above applicable height if enclosed by parapets or walls</p> <p>4. Rooftop features below may extend up to 15 ft above applicable height as long as combined total coverage, including eaves and canopies, does not exceed 20% of roof area, or 25% if total includes stair or elevator penthouses or screened mechanical equipment. Features include: solar collectors, mechanical equipment, stair and elevator penthouses up to 16 ft</p> <p>7. Rooftop features shall be located at least 10 ft from the north lot line unless a shadow diagram is provided that demonstrates these features would not shade property to the north on Jan 21st at noon more than a structure built to max height and FAR</p>	<p>Elevator penthouse extends up to 16 ft above the maximum height.</p>

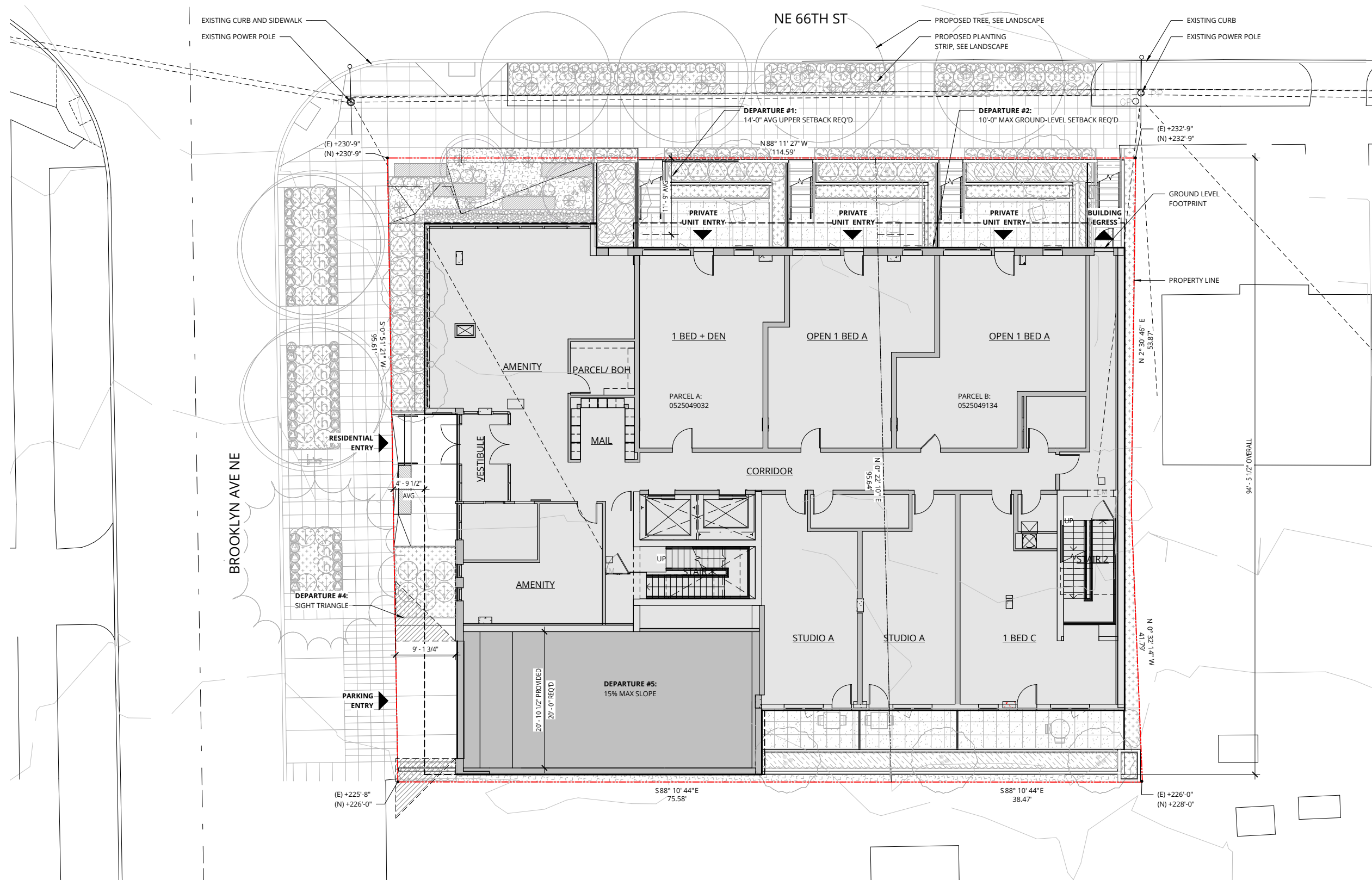
CODE SECTION		RESPONSE
<b>23.47A.013 FLOOR AREA RATIO</b>	Section A: All gross floor area not exempt in 23.47a.013.d is counted in FAR Table B: Max 5.75 FAR, FAR exceeding incentive zoning suffix to comply with SMC 23.58a Section D: Gross floor area not counted against FAR All underground stories or portions of stories All portions of a story that extend no more than 4 ft above existing or finished grade, whichever is lower	Project FAR is 4.91
<b>23.47A.014 SETBACK REQUIREMENTS</b>	Section E: Structures in required setbacks 2. Eaves, cornices and gutters projecting up to 18 inches are allowed; 3. Ramps for access for disabled and elderly; 5. Fences up to 6 ft in height are allowed; 6. Setback requirements do no apply to underground structures; 8. Dumpsters, except for compactors, must be screened; 9-10. Green stormwater infrastructure is allowed with restrictions	Fences below 6 ft are proposed in setback along NE 66th St
<b>23.47A.016 LANDSCAPING STANDARDS</b>	Section A2: Green factor of >/= 0.30 required Section B: Street trees required, existing street trees to be retained unless approved by director Table D: Garbage dumpsters require a 6 ft high screen	Street trees will be provided, see landscape plan.
<b>23.47A.022 LIGHT STANDARDS</b>	Section A. Exterior lighting must be shielded Section B. Interior lighting in parking garages must be shielded to minimize nighttime glare	Lighting will be shielded
<b>23.47A.024 AMENITY AREA</b>	Section A: amenity areas required equal to 5% of the total gfa in residential use, exclude mechanical and accessory parking. Bioretention facilities qualify Section B: All residents shall have access to at least 1 amenity area. Amenity areas shall not be enclosed. Common areas shall have a min horizontal area of 10 ft and shall be a min of 250 sf in size. Private balconies and decks shall be min 60 sf and no horizontal dimension less than 6 ft	Roof level common amenity and roof level and ground level private amenity areas are provided.
<b>23.47A.032 PARKING ACCESS</b>	Section A: in NC zones, if the lot abuts 2 or more streets, access is permitted per section 23.47a.032.c and curb cuts per 23.54.030.f.2.a.1. One garage door not to exceed the width of the curb cut allowed per each permitted curb cut. Section C: The director will determine which of the streets will be considered the front lot line	Proposed parking access is from Brooklyn Ave NE.
<b>23.54.015 REQUIRED PARKING</b>	Table B: No parking required for residential uses in urban centers Table D: Multifamily structures - 1/4 long term space per each unit Section K: Bicycle parking: Required bicycle parking shall be provided in a safe, accessible and convenient location. All required long term parking shall be covered because automobile parking is covered. Must be separated from automobile parking with barrier or painted lines	Separated long term bicycle parking is provided in levels P1 and P2. 77 units / 4 = 20 req'd spaces. 21 spaces are provided.
<b>23.54.030 PARKING SPACE STANDARDS</b>	Section D: Driveways. Driveway shall be min of 20 ft wide for two way traffic and driveway slope shall be 15% unless approved by the director Section F: Curb Cuts. Frontage along Brooklyn ave ne is between 80 ft and 160 ft, therefore 2 curb cuts are allowed, (1) 20 ft wide curb cut substituted for (2) 10 ft wide. A curb cut flare of 2.5 ft is permitted on either side of curb cuts Section G: Sight Triangles. A sight triangle 10 ft from the sidewalk shall be kept clear between 32-82 inches from the ground When the driveway is less than 10 ft from the lot line, an easement shall be recorded to maintain the sight triangle. The driveway may begin 5 ft from the lot line	See appendix for additional information.  See appendix for additional information.





EXISTING SURVEY





SITE PLAN



GUIDANCE THEME	BOARD GUIDANCE	DESIGN RESPONSE	APPLICABLE DESIGN GUIDELINE	
1. MASSING & DESIGN CONCEPT	<p>The Board generally supports the preferred massing option.</p> <ul style="list-style-type: none"><li>• Provide perspectives and sections that show neighborhood context, including the high school.</li><li>• Show relationship of proposed design in relation to other recently approved projects, show colors, textures, materials and forms.</li></ul>	<p>See the sections and perspectives that show neighborhood context, including the high school. The concept of a two sided character is unique in this area. The facade on Brooklyn Ave NE is sleeker and more urban in character while the building is more residential in character facing NE 66th St with private residential patios and decks.</p>	DC1-B-1 DC2-A-2 DC2-C-1 DC4-A-1	Access Location and Design Reducing Perceived Mass Visual Depth and Interest Exterior Finish Materials
2. GROUND LEVEL SETBACK	<p>The Board supports the general concept of creating opportunities for more activity on NE 66th St.</p> <ul style="list-style-type: none"><li>• The Board was concerned that the patios might be too large.</li><li>• The Board was questioned whether the stepped down private patios is preferred to a more urban stoop.</li><li>• The Board requested sections and other information to prove this is the best design, include a long section of the street.</li></ul>	<p>Additional sections and images are provided. Stepped down residential patios are preferred on NE 66th St to tie in neighborhood precedent including the neighboring building to the west. Ground level patios allow more natural light into the ground level units than a raised, urban stoop would. Furthermore, 3 larger patios provide more usable and activated exterior patio space than an urban stoop typology would (small landings to allow light into ground level units).</p>	CS2-D-1 CS3-I-ii PL3-B-2	Existing Development and Zoning Vibrant Streetscape Ground Level Residential
3. SIDEWALK WIDTH & ADJACENT BLOCK TRANSITION	<ul style="list-style-type: none"><li>• The Board requested more space be given to the sidewalk along NE 66th St, more in line with SDOT recommendation of 8-ft wide sidewalk and 6-ft wide planter.</li><li>• The Board requested the residential patios along NE 66th St be reduced in depth.</li></ul>	<p>The sidewalk width along NE 66th St is in line with SDOT's recommendation of providing an 8-ft wide sidewalk. Residential patios are reduced in depth. Landscape plans with neighboring context are provided to show sidewalk transition.</p>	PL1-B-2 PL2-B-1	Pedestrian Volumes Eyes on the Street
4. LANDSCAPE PLAN	<ul style="list-style-type: none"><li>• The Board questioned the Douglas fir but could support this selection if composition and relationship to the project was well conceived.</li><li>• The Board suggested providing an arborist report for existing street trees.</li></ul>	<p>Updated renderings are provided to show the composition and relationship of the douglas fir to the project. The placement of the douglas fir will help define the outdoor room at the building entry and aligns with the neighborhood street tree plan. An arborist report summary is included in section 10 Landscape/Hardscape Plan.</p>	CS3-II-i DC4-D-1 DC4-D-4 DC4-IV-i DC4-IV-iii	Private Open Spaces Choice of Plant Materials Place Making Historical Landscape Elements Indigenous Trees
5. ENTRY TRANSITION	<ul style="list-style-type: none"><li>• The Board was concerned about the lack of a distinct building entrance on the corner but were satisfied with how the mid-block entry leads directly into the lobby area defined by the two-story transparent corner.</li></ul>	<p>The building entrance is located on Brooklyn Ave NE to align with existing grades and to allow for an outdoor room adjacent douglas fir tree. The building is setback from the northwest corner of the site to provide additional space for pedestrians as well as a public seating bench. Storefront glazing allows views into the 2 story residential lobby space accessed directly from the building entrance located on Brooklyn Ave NE. See updated images.</p>	CS2-B-2 CS2-C-1 PL2-B-3	Connection to the Street Corner Sites Street Level Transparency
6. GARAGE DOOR	<ul style="list-style-type: none"><li>• The Board suggested the replication/preservation of the Pacific Northwest Native American art motif painted on the existing garage on the site.</li></ul>	<p>Noted garage door has been removed from the site by the previous owner.</p>	CS3-B-2	Historical/Cultural References
7. CONTEXT	<ul style="list-style-type: none"><li>• The Board asked for additional exhibits which show larger neighborhood context. Show what makes this design unique and how does it fit in neighborhood in 5 years.</li></ul>	<p>In addition to the sections and perspectives provided in response to guidance 1, perspectives of neighboring proposed projects and streetscape elevations with adjacent proposed projects are provided for context. The immediate neighborhood character is rapidly changing into a vibrant pedestrian zone with the future light rail station. Surrounding planned uses are primarily residential with retail focused on NE 65th St.</p>	CS2-I-ii CS3-I-i DC2-C-3 DC4-I-iii	Fabric of Connected Buildings Roosevelt High School Fit with Neighboring Buildings Colors



EDG MASSING



**EDG:**  
GROSS SQUARE FOOTAGE: APPROXIMATELY **52,800 SF**  
7 LEVELS ABOVE GRADE  
71 UNITS  
38 PARKING SPACES

- DESIGN FEATURES**
- LEVEL 1 & 2 - 14' SETBACK ON NE 66TH ST
  - LEVEL 1 & 2 - 9'-8" SETBACK ON BROOKLYN AVE NE
  - RESIDENT LOBBY AND AMENITY SPACES AND A ROOF DECK

- DEVELOPMENT OBJECTIVES**
- PROVIDE A HIGH QUALITY LIVING ENVIRONMENT FOR RESIDENTS WITH ACCESS TO PUBLIC TRANSPORTATION, RETAIL, RESTAURANTS, AND OUTDOOR AMENITIES
  - PROVIDE A PEDESTRIAN-ORIENTED STREETFRONT ON NE 66TH ST (WIDEN SIDEWALK AND PLANT STREET TRESS, APPROPRIATELY SCALED FACADE)
  - PROVIDE A RESIDENT-ORIENTED STREETFRONT ON BROOKLYN AVE NE (RESIDENTIAL ENTRY TO PROVIDE EYES ON THE STREET, LANDSCAPE TO BLEND WITH NEIGHBORS, SAFE AND ATTRACTIVE PARKING ENTRY, LOCATE TRASH AND RECYCLING STORAGE OUT OF SIGHT)

**MUP:**  
GROSS SQUARE FOOTAGE (FAR): **56,864 SF**  
7 LEVELS ABOVE GRADE  
77 UNITS  
43 PARKING SPACES

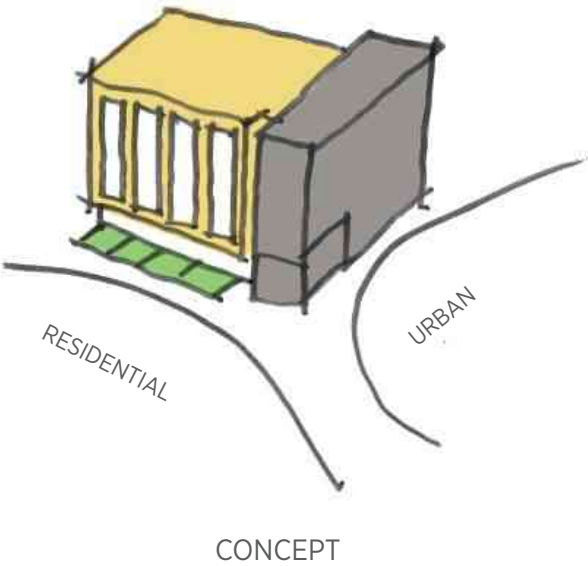
PROPOSED DESIGN



**DRB:**  
GROSS SQUARE FOOTAGE: **53,360 SF**  
7 LEVELS ABOVE GRADE  
77 UNITS  
43 PARKING SPACES BELOW GRADE

- PRIMARY DESIGN FEATURES**
- LEVEL 1 & 2 - 13'-9" SETBACK ON NE 66TH ST
  - LEVEL 1 & 2 - 9'-8" SETBACK ON BROOKLYN AVE NE
  - RESIDENT LOBBY AND AMENITY SPACES AND A ROOF DECK

- DEVELOPMENT OBJECTIVES**  
GOALS AT EDG APPLY, IN ADDITION:
- PROVIDE EYES ON THE STREET ON NE 66TH ST (INDIVIDUAL GROUND LEVEL PATIOS AND UNIT DECKS)
  - PROVIDE A WELL PROPORTIONED BASE
  - FIT WITH SURROUNDING PROPOSED ADJACENT DEVELOPMENTS





8.0 ITEMIZED EDG RESPONSE - 1. MASSING AND DESIGN CONCEPT

BOARD GUIDANCE:

The Board was generally supportive of the preferred massing option (Alternative 3) but echoed public comments about wanting to see how this design proposal fits into the overall context of the neighborhood in through the use of colors, textures, materials, and forms.

a. The Board requested that the applicant provide more images; perspectives, sections that show the overall neighborhood context, including the high school, of the proposed design in relationship to other recently approved projects in the area. The Board was keenly interested in how this project, while being brought forward by the same developer as several other projects in the area, will be different from the others and what will make it unique.

RESPONSE:

See the sections and perspectives that show neighborhood context, including the high school. The concept of a two sided character is unique in this area. The facade on Brooklyn Ave NE is sleeker and more urban in character while the building is more residential in character facing NE 66th St with private residential patios and decks.

RELEVANT GUIDELINES:

**DC1-B-1. Access Location and Design:** Emphasize use of the sidewalk for pedestrians and create safe and attractive conditions for pedestrians, bicyclists, and drivers: choose a location for street access that is the least visually dominant and/or which offers opportunity for shared driveway use and employ a multi-sensory approach to areas of potential vehicle pedestrian conflict.

**DC2-A-2. Reducing Perceived Mass:** Use secondary architectural elements to reduce the perceived mass of larger projects. Consider creating recesses or indentations in the building envelope and/or highlighting building entries.

**DC2-C-1. Visual Depth and Interest:** Add depth to facades where appropriate by incorporating balconies, canopies, awnings, decks.

**DC4-A 1. Exterior Finish Materials:** Building exteriors should be constructed of durable and maintainable materials that are attractive even when viewed up close.





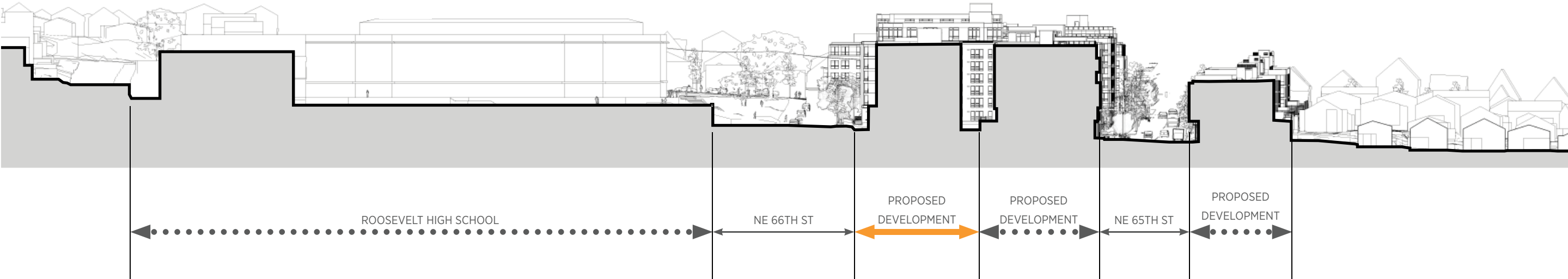




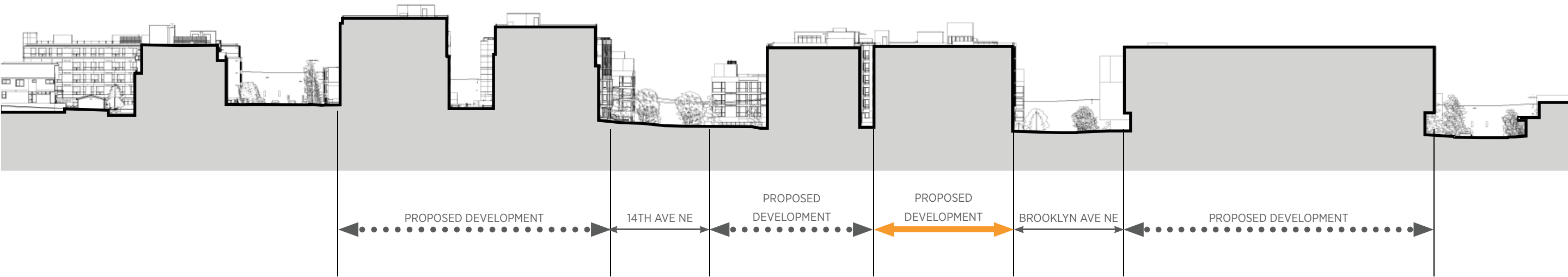
8.0 ITEMIZED EDG RESPONSE - 1. MASSING AND DESIGN CONCEPT







NORTH-SOUTH SECTION WITH SURROUNDING PROPOSED CONTEXT



EAST-WEST SECTION WITH SURROUNDING PROPOSED CONTEXT



8.0 ITEMIZED EDG RESPONSE - 2. GROUND LEVEL SETBACK

BOARD GUIDANCE:

Board members were surprised how the building frontage along 66th Street was set back away from the property line and then stepped down to private patios and questioned why the use of this typology is preferred as opposed to the reverse as would be seen in a more urban stoop type of condition. Board members were concerned that the patio spaces along 66th St. might be excessively large but supported the general concept of creating opportunities for more activity along 66th with the larger than usual building setback.

- a. The Board requested additional sections and other information to demonstrate how this approach is the best design solution.
- b. The Board requested a long section drawing of the street to gain a better understanding of how the sidewalk, setbacks, landscaping, patio and other elements work.

RESPONSE:

Additional sections and images are provided. Stepped down residential patios are preferred on NE 66th St to tie in neighborhood precedent including the neighboring building to the west. Ground level patios allow more natural light into the ground level units than a raised, urban stoop would. Furthermore, 3 larger patios provide more usable and activated exterior patio space than an urban stoop typology would (small landings to allow light into ground level units).

RELEVANT GUIDELINES:

CS2-D-1. Existing Development and Zoning:

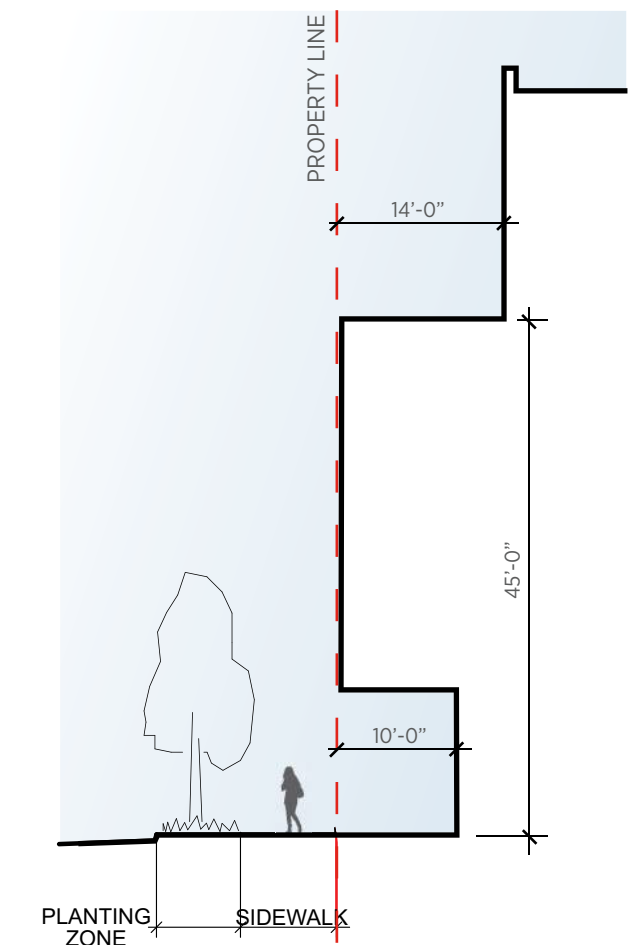
Review the height, bulk, and scale of neighboring buildings and future buildings to design an appropriate complement.

CS3-I-ii. Vibrant Streetscape:

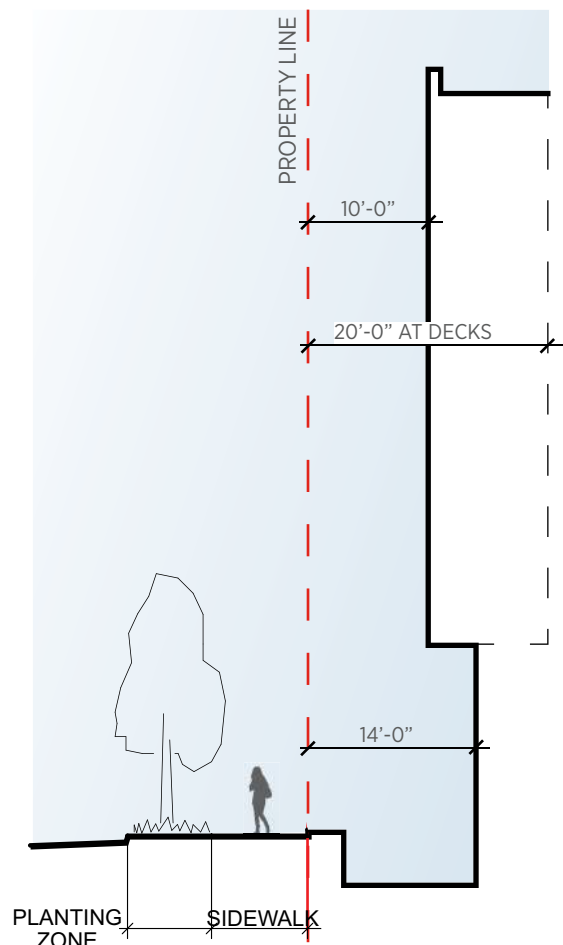
Reinforce a vibrant streetscape: apply a pedestrian-oriented design and include multiple recessed entries

PL3-B-2. Ground Level Residential:

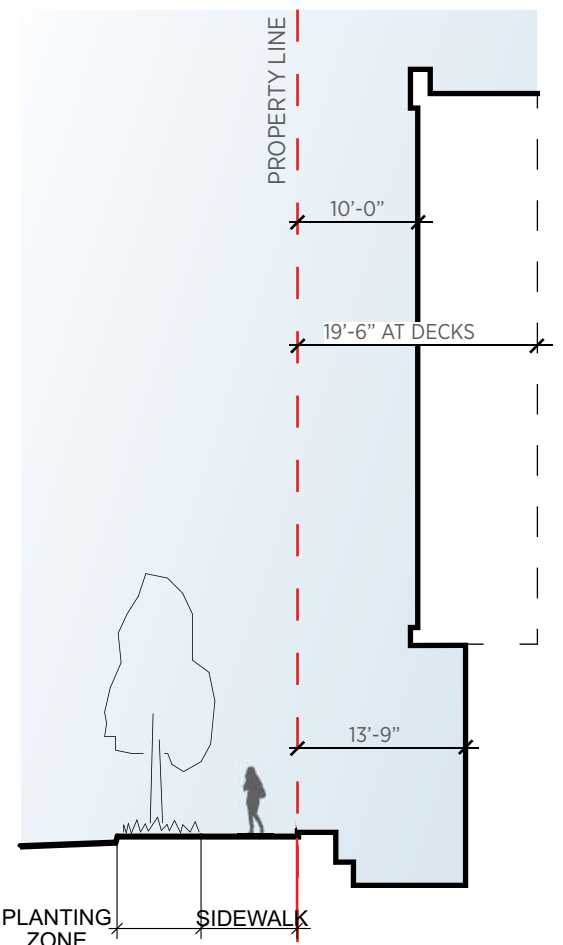
Privacy and security issues are important in buildings with ground-level housing. Consider providing a greater number of transition elements and spaces, and choose materials carefully to clearly identify the transition from public sidewalk to private residence. Vertical modulation and finishes on the facade can articulate the location of residential entries; use entry elements such as entry lights, planter boxes or pots.



A. CODE MINIMUM SECTION AT NE 66TH ST



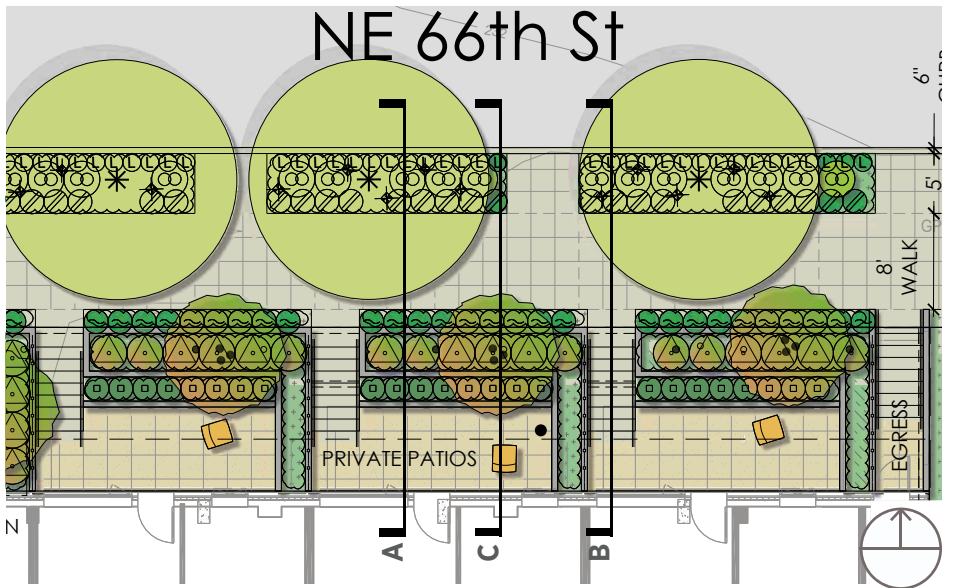
A. SECTION AT NE 66TH ST - EDG PREFERRED ALTERNATE 3



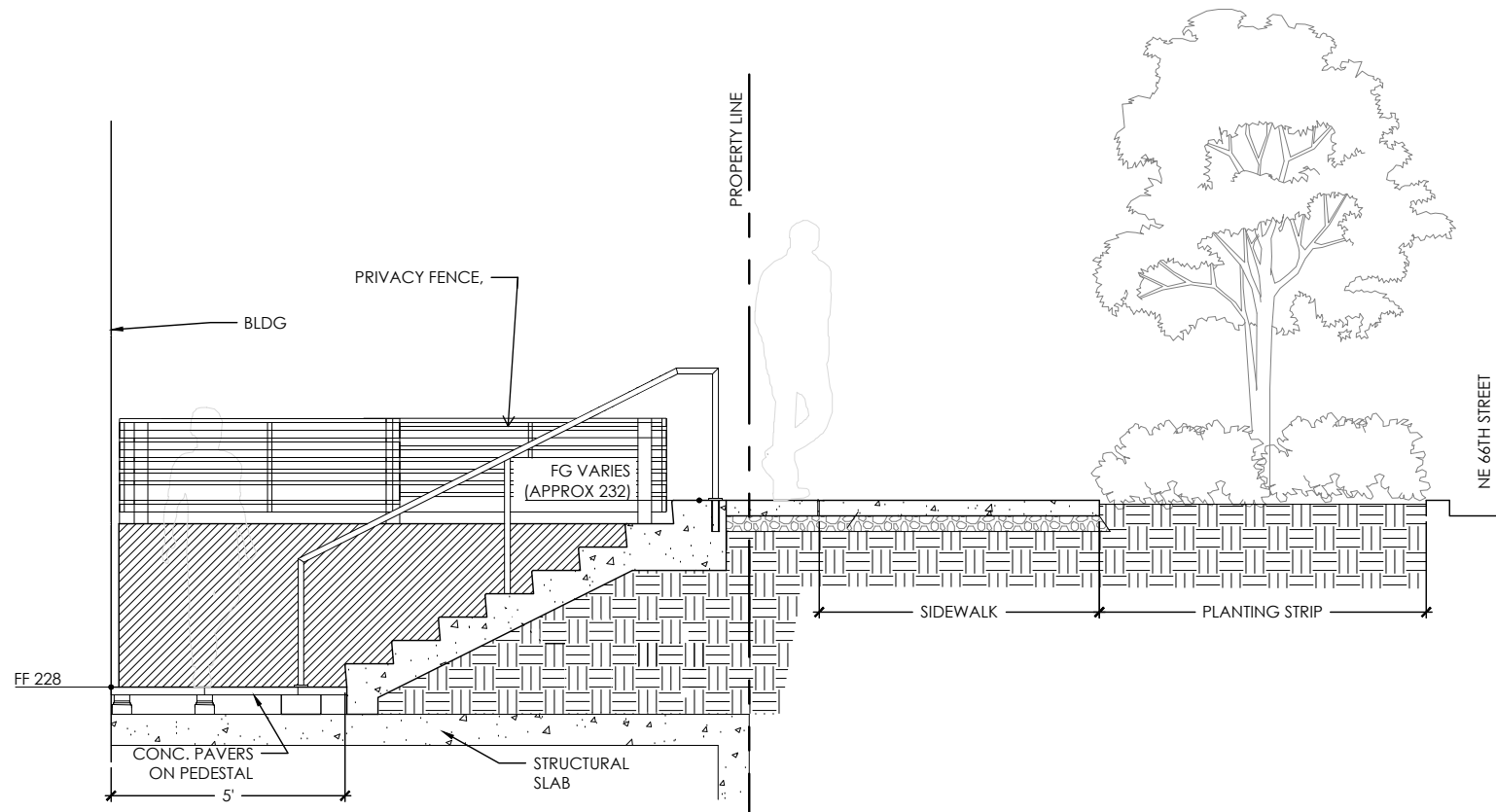
A. PROPOSED SECTION AT NE 66TH ST



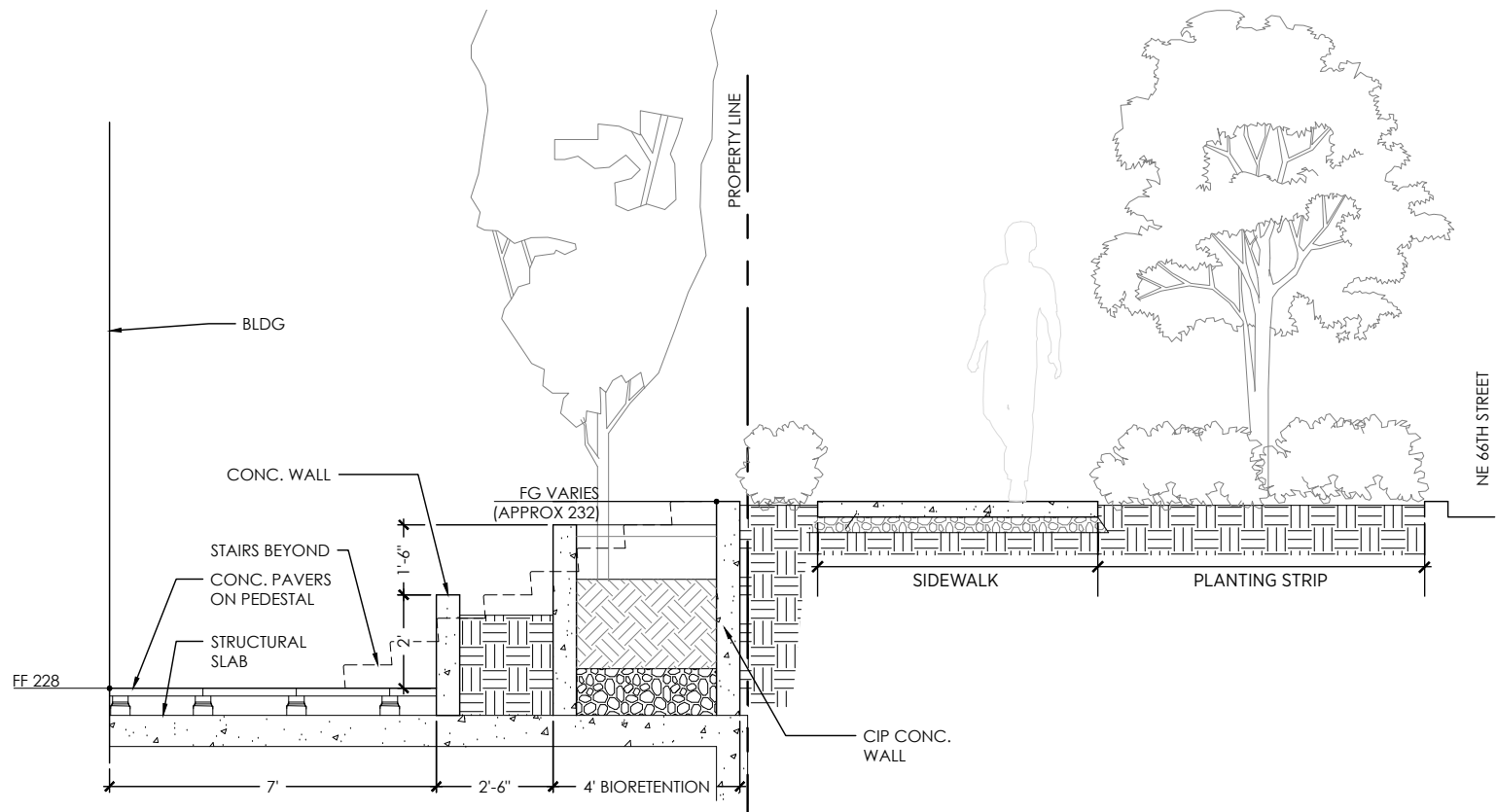
EXAMPLE OF STEPPED DOWN RESIDENTIAL PATIOS







B. ENLARGED SECTION AT RESIDENTIAL PATIO STAIRS ON NE 66TH ST



C. ENLARGED SECTION AT RESIDENTIAL PATIOS ON NE 66TH ST



NEIGHBORHOOD EXAMPLE: PROPOSED STREETSCAPE ON NE 66TH ST



PROPOSED STREETSCAPE ON NE 66TH ST



8.0 ITEMIZED EDG RESPONSE - 3. SIDEWALK WIDTH AND ADJACENT BLOCK TRANSITION

BOARD GUIDANCE:

Board members were curious about the width of the sidewalks of the proposal and how it transitions to the sidewalks of the adjacent project to the west.

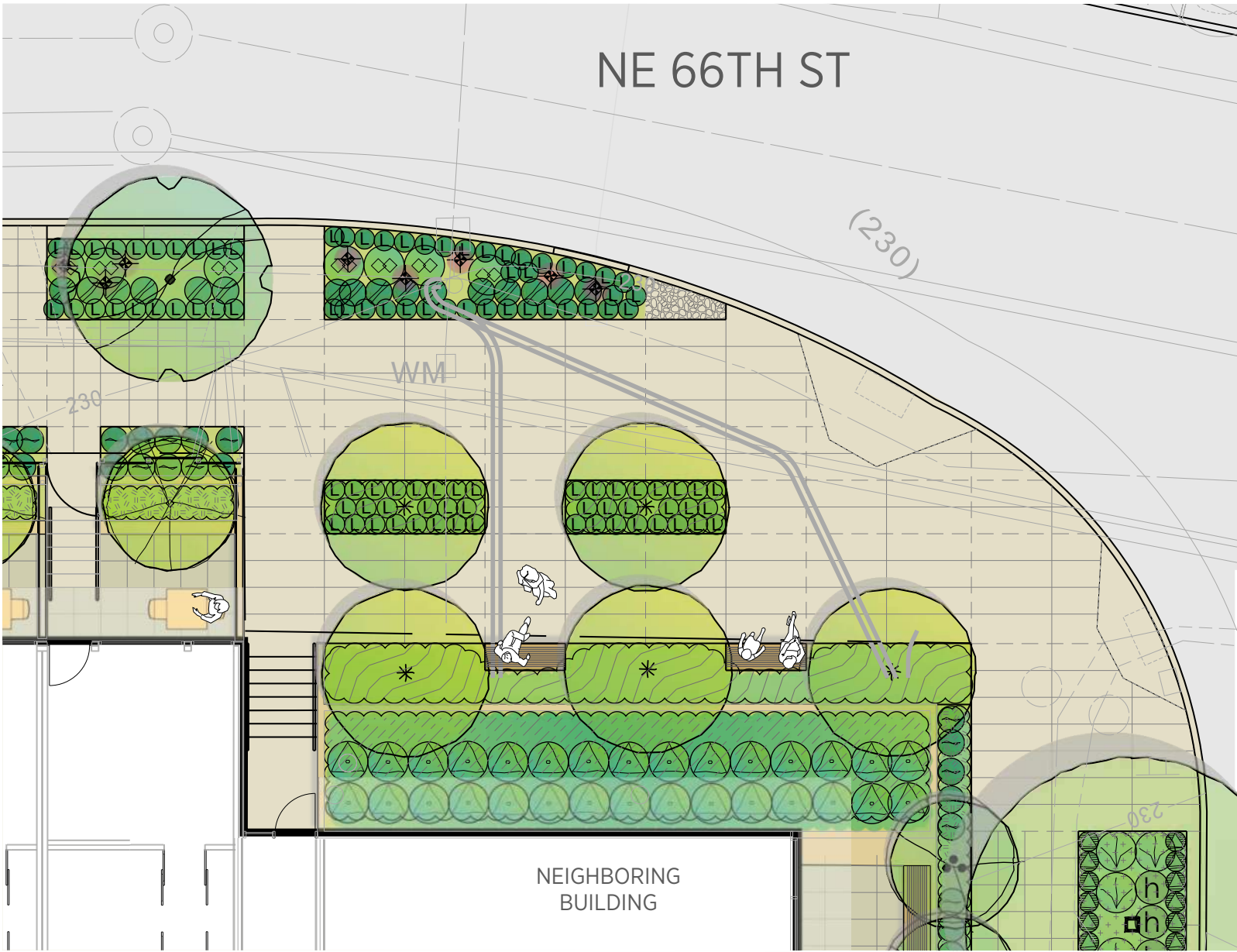
- a. The Board agreed with public comment and requested that more space be given to the sidewalk width along 66th St., more in line with Seattle Department of Transportation’s (SDOT) recommendation of providing an 8-foot wide sidewalk and a 6-foot wide planter. The Board agreed that one way to achieve this would be to start the landscaping within the property line instead of it protruding forward toward the street, thus allowing more space for sidewalk use.
- b. The Board requested that the residential patios along 66th St. be reduced in depth to allow for additional space to be allocated to the sidewalk width.

RESPONSE:

The sidewalk width along NE 66th St is in line with SDOT’s recommendation of providing an 8-ft wide sidewalk. Residential patios are reduced in depth. Landscape plans with neighboring context are provided to show sidewalk transition.

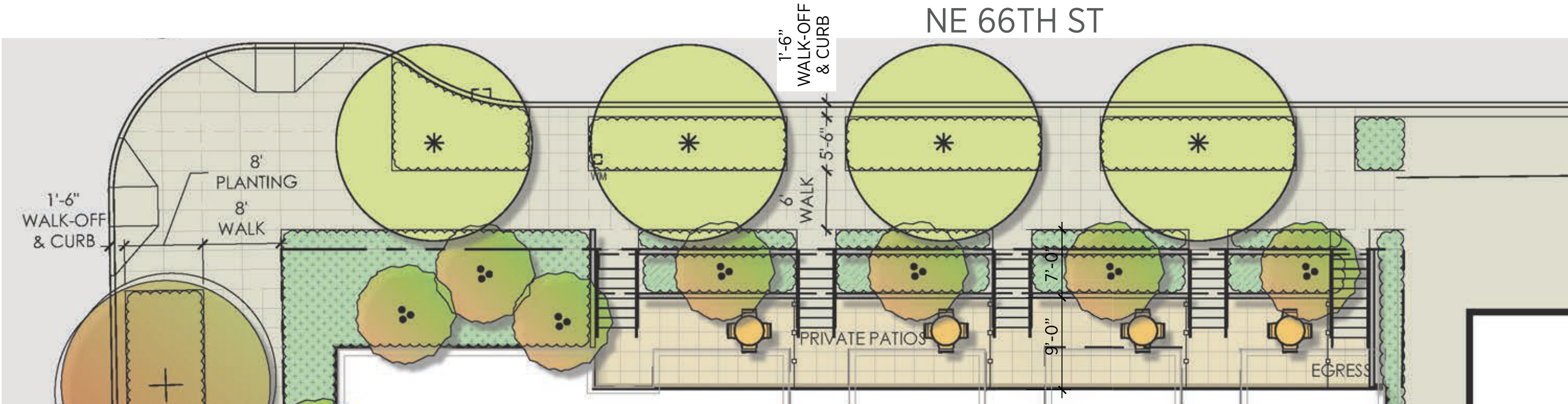
RELEVANT GUIDELINES:

- PL1-B-2. Pedestrian Volumes:**  
Provide ample space for pedestrian flow and circulation, particularly in areas where there is already heavy pedestrian traffic or where the project is expected to add or attract pedestrians to the area.
- PL2-B-1. Eyes on the Street:**  
Create a safe environment by providing lines of sight and encouraging natural surveillance through strategic placement of doors, windows, balconies and street-level uses.



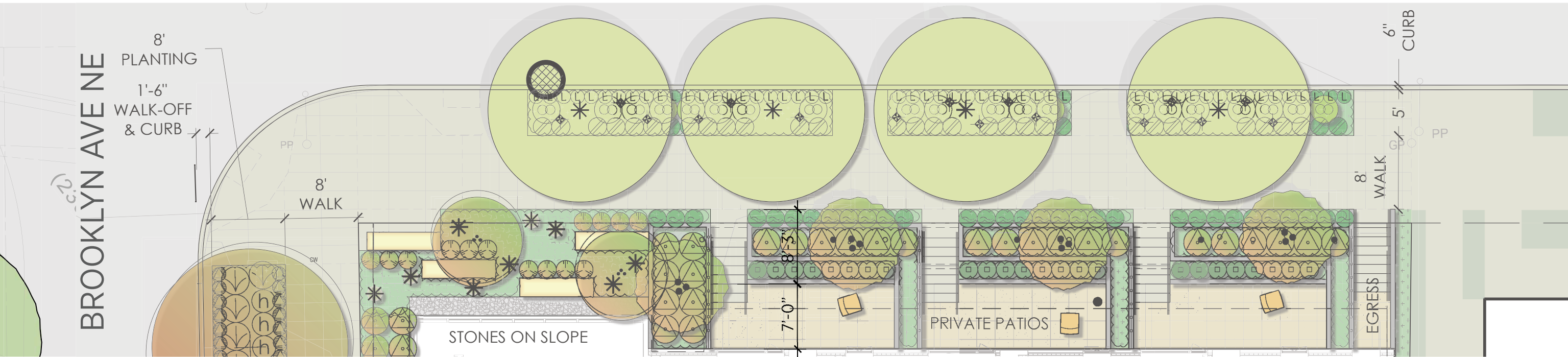


NE 66TH ST



EDG - NE 66TH ST STREETScape AND RESIDENTIAL PATIOS

NE 66TH ST



NE 66TH ST STREETScape, RESIDENTIAL PATIOS AND SIDEWALK TRANSITION



BOARD GUIDANCE:

The Board questioned the use of the Douglas fir as a street tree as they are normally forest trees. In light of approval by the City’s arborist and public comment, the Board could support this selection if the composition and relationship to the project design was well conceived.

- a. The Board suggested that the applicant provide an arborist report done to assess the current disposition of the existing street trees.

RESPONSE:

Updated renderings are provided to show the composition and relationship of the douglas fir to the project. The placement of the douglas fir will help define the outdoor room at the building entry and aligns with the neighborhood street tree plan. An arborist report summary is included in section 10 Landscape/Hardscape Plan.

RELEVANT GUIDELINES:

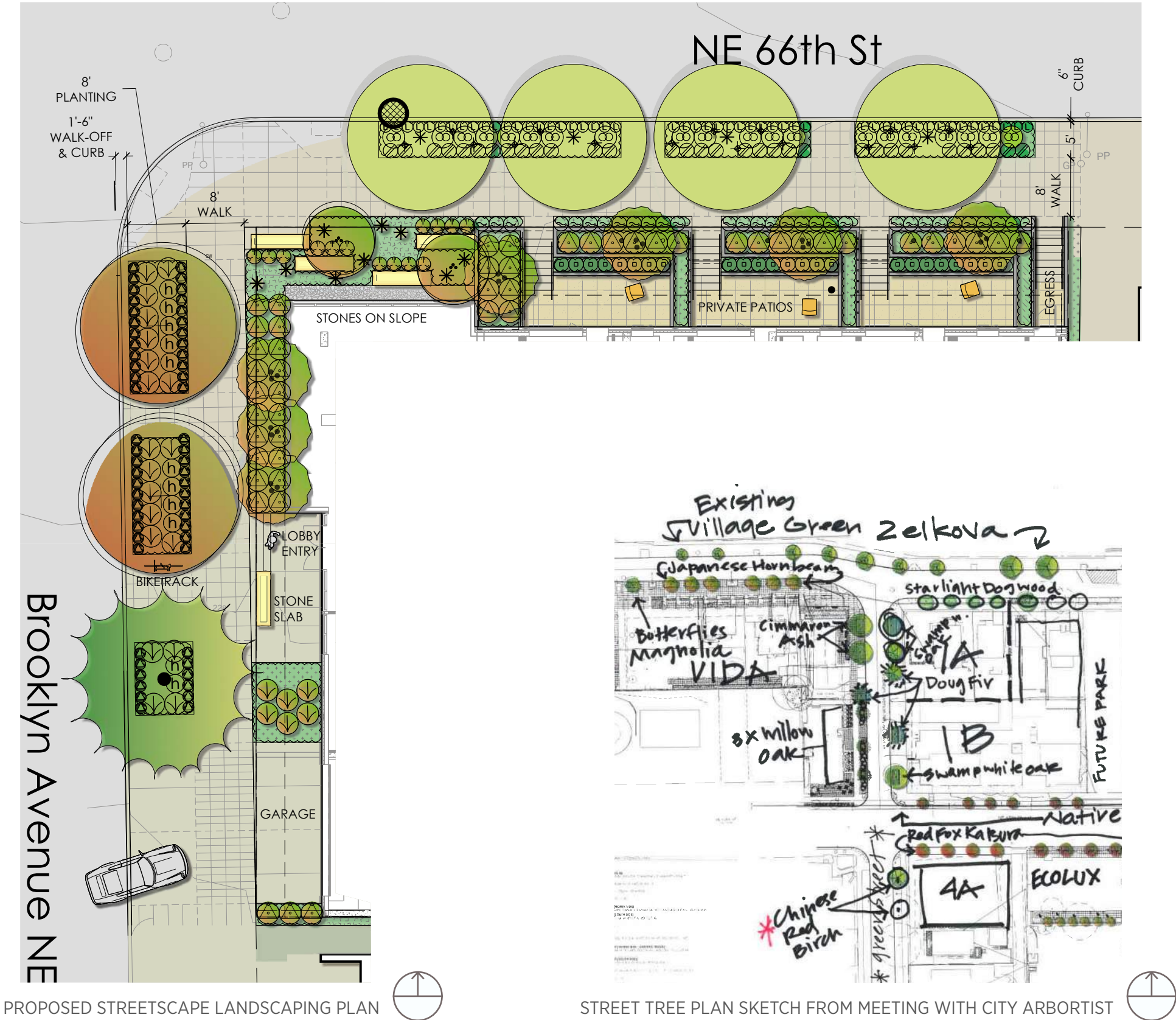
**CS3-II-i. Private Open Spaces:** Consider incorporating private open spaces between the street and residences and between adjacent properties.

**DC4-D 1. Choice of Plant Materials:** Reinforce the overall architectural and open space design concepts through the selection of landscape materials. Choose plants that will emphasize or accent the design, create enduring green spaces, and be appropriate to particular locations taking into account solar access, soil conditions, and adjacent patterns of use. Select landscaping that will thrive under urban conditions.

**DC4-D-4. Place Making:** Create a landscape design that helps define spaces with significant elements such as trees.

**DC4-IV-i. Historical Landscape Elements:** Neighborhood plant choices should consider historical landscape elements.

**DC4-IV-iii. Indigenous Trees:** Indigenous trees should be planted to maintain and reinvigorate a verdant tree canopy within the neighborhood.







PROPOSED STREETScape - VIEW FROM THE NORTHWEST



PROPOSED STREETScape - VIEW FROM THE SOUTH



BOARD GUIDANCE:

Board members were concerned about the lack of a distinct building entrance at the corner of the preferred option but were satisfied with how the mid-block entry along Brooklyn Ave. leads directly into the lobby area defined by the large two-story transparent façade, located at the corner and in effect announcing the primary entry into the building.

RESPONSE:

The building entrance is located on Brooklyn Ave NE to align with existing grades and to allow for an outdoor room adjacent douglas fir tree. The building is setback from the northwest corner of the site to provide additional space for pedestrians as well as a public seating bench. Storefront glazing allows views into the 2 story residential lobby space accessed directly from the building entrance located on Brooklyn Ave NE. See updated images.

RELEVANT GUIDELINES:

**CS2-B-2. Connection to the Street:**  
Identify opportunities for the project to make a strong connection to the street and carefully consider how the building will interact with the public realm. Consider the qualities and character of the streetscape (sidewalk, parking, landscape strip, street trees, travel lanes, and other amenities) and its function in siting and designing the building.

**CS2-C-1. Corner Sites:**  
Corner sites can serve as gateways or focal points; both require careful detailing at the first three floors due to their high visibility from two or more streets and long distances. Consider using a corner to provide extra space for pedestrians and a generous entry, or build out to the corner to provide a strong urban edge to the block.

**PL2-B-3. Street Level Transparency:**  
Ensure transparency of street-level uses (for uses such as nonresidential uses or residential lobbies), by keeping views open into spaces at corners.



PROPOSED CORNER AT EDG



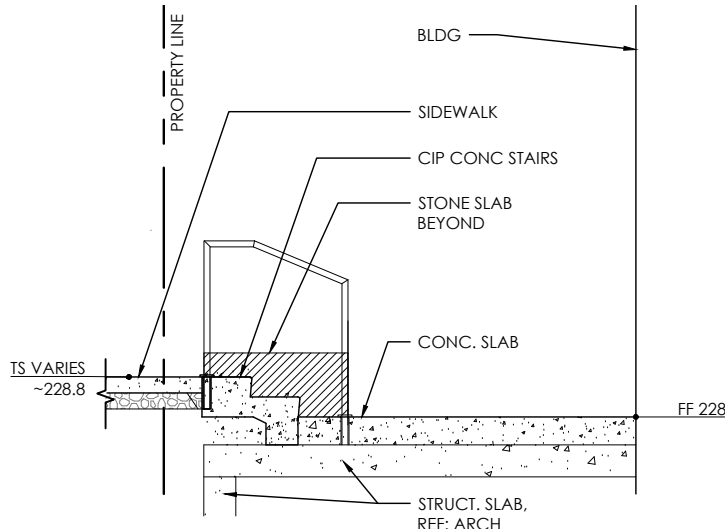
PROPOSED CORNER



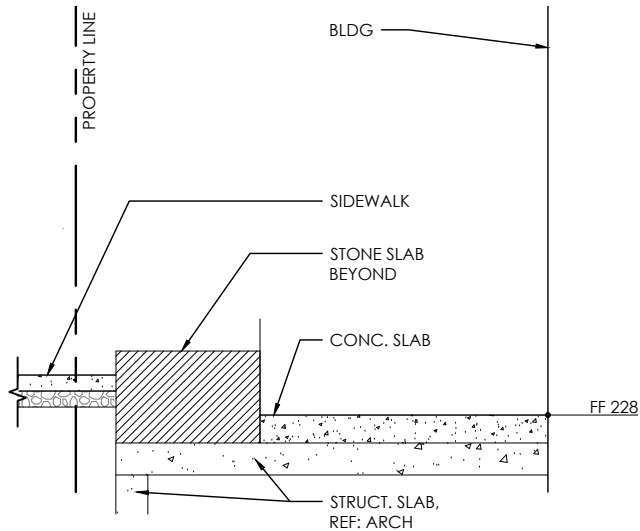
8.0 ITEMIZED EDG RESPONSE - 5. ENTRY TRANSITION



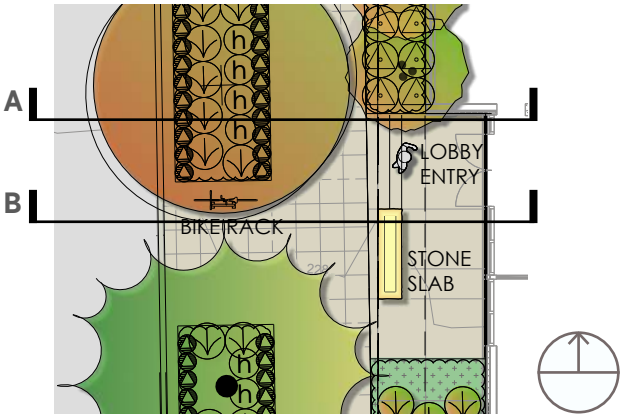
PROPOSED ENTRY



A. STEPS AT BUILDING ENTRY



B. SEATING AT BUILDING ENTRY





8.0 ITEMIZED EDG RESPONSE - 6. GARAGE DOOR

BOARD GUIDANCE:

As a humanizing design element, the Board reiterated public comment and suggested the replication/preservation of the Pacific Northwest Native American art motif painted on single-family residential garage located on the southwest corner of the proposal site as an homage to the previous image.

RESPONSE:

Noted garage door has been removed from the site by the previous owner.

RELEVANT GUIDELINES:

CS3-B-2. Historical/Cultural References:

Reuse existing structures on the site where feasible as a means of incorporating historical or cultural elements into the new project.



REFERENCED GARAGE DOOR



CURRENT GARAGE DOOR - PAINTED DOOR WAS REMOVED BY PREVIOUS OWNER



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BOARD GUIDANCE:

The Board asked that the applicant to provide more exhibits that show the larger neighborhood context and what makes it unique in addition to images of what the context of the neighborhood will look like in the next 5 years.

RESPONSE:

In addition to the sections and perspectives provided in response to guidance 1, perspectives of neighboring proposed projects and streetscape elevations with adjacent proposed projects are provided for context. The immediate neighborhood character is rapidly changing into a vibrant pedestrian zone with the future light rail station. Surrounding planned uses are primarily residential with retail focused on NE 65th St.

RELEVANT GUIDELINES:

- CS3-I-ii. Fabric of Connected Buildings:**  
Develop a fabric of connected buildings through streetscapes.
- CS3-I-i. Roosevelt High School Architectural Heritage:**  
Roosevelt High School Architectural Heritage: New buildings built adjacent to the high school should complement and defer to the architectural prominence of the school, and contribute to a campus-like setting.
- DC2-C-3. Fit With Neighboring Buildings:** Use design elements to achieve a successful fit between a building and its neighbors, such as:

a. architectural style, roof line, datum line detailing, fenestration, color or materials,

b. using trees and landscaping to enhance the building design and fit with the surrounding context, and

c. creating a well-proportioned base, middle and top to the building in locations where this might be appropriate. Consider how surrounding buildings have addressed base, middle, and top.
- DC4-I-iii. Colors:** Colors should be consistent with and chosen based on existing architectural cues and should be considered in terms of their relationship to neighboring structures.



LEGEND

1. Future 7-Story Mixed-Use Project (under construction)

2. Future 6-Story Mixed-Use Project (under construction)

3. Future 7-Story Mixed-Use Project

4. Future 7-Story Mixed-Use Project

5. Future 4-Story Mixed-Use Project
6. 4-Story Mixed-Use Project

7. Future 4-Story Mixed-Use Project

8. Future 5-Story Mixed-Use Project

9. Roosevelt High School

FUTURE SURROUNDING CONTEXT AXONOMETRIC VIEW



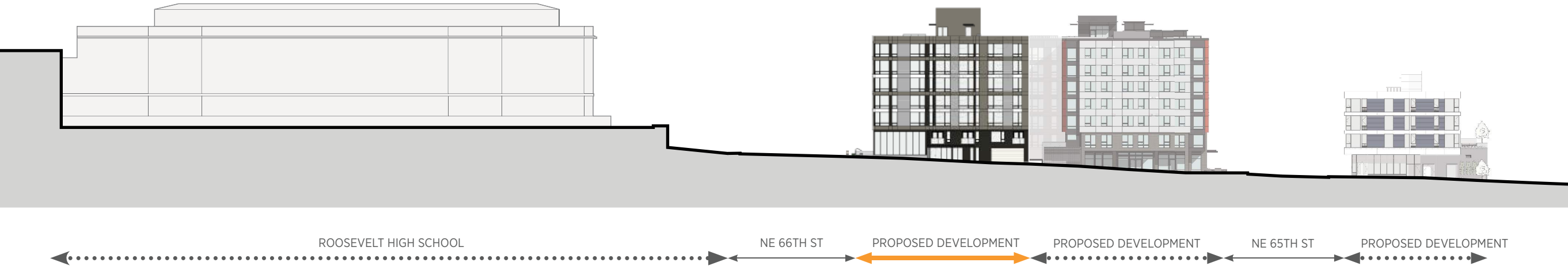






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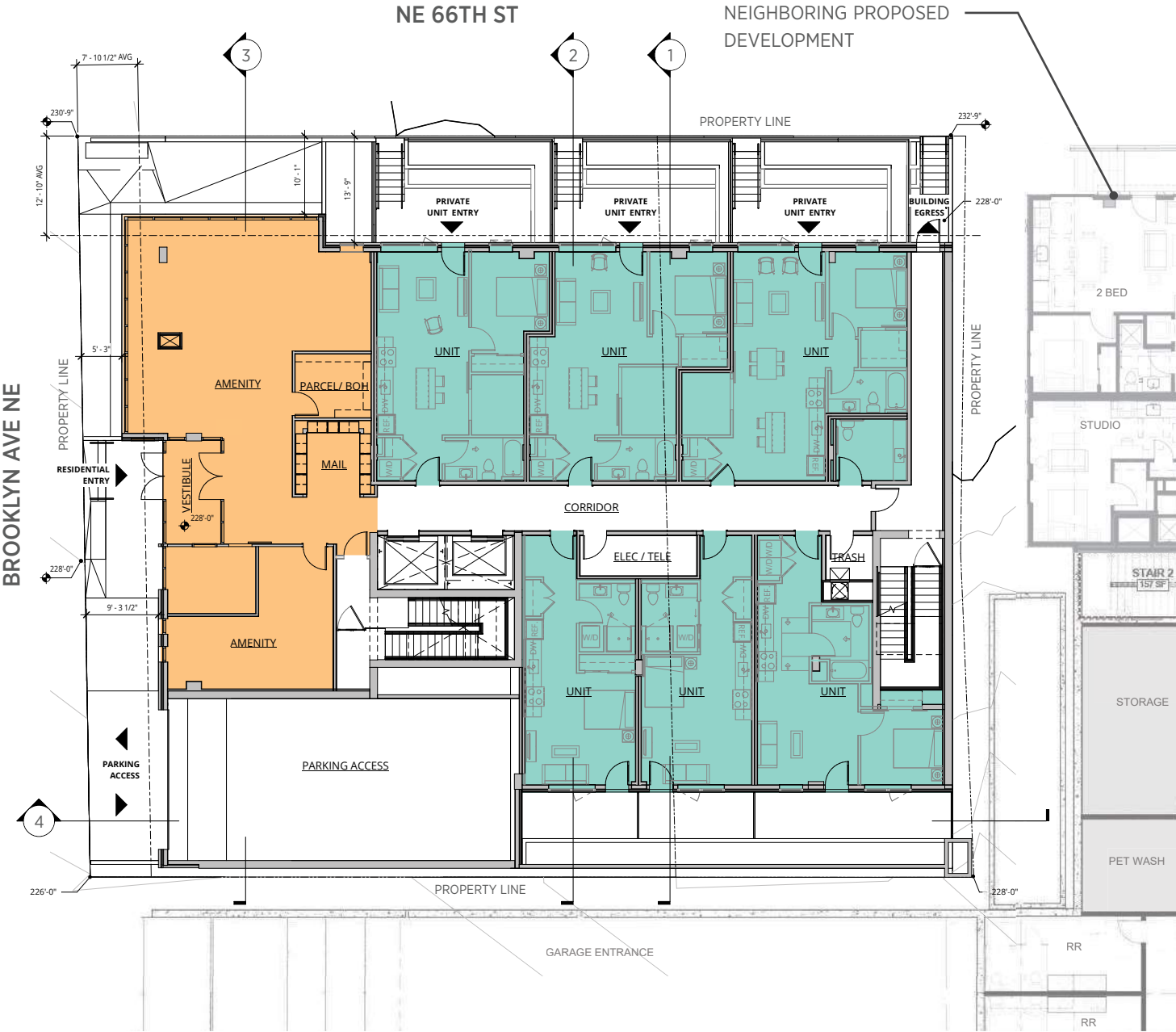
WEST ELEVATION WITH SURROUNDING PROPOSED CONTEXT



NORTH ELEVATION WITH SURROUNDING PROPOSED CONTEXT



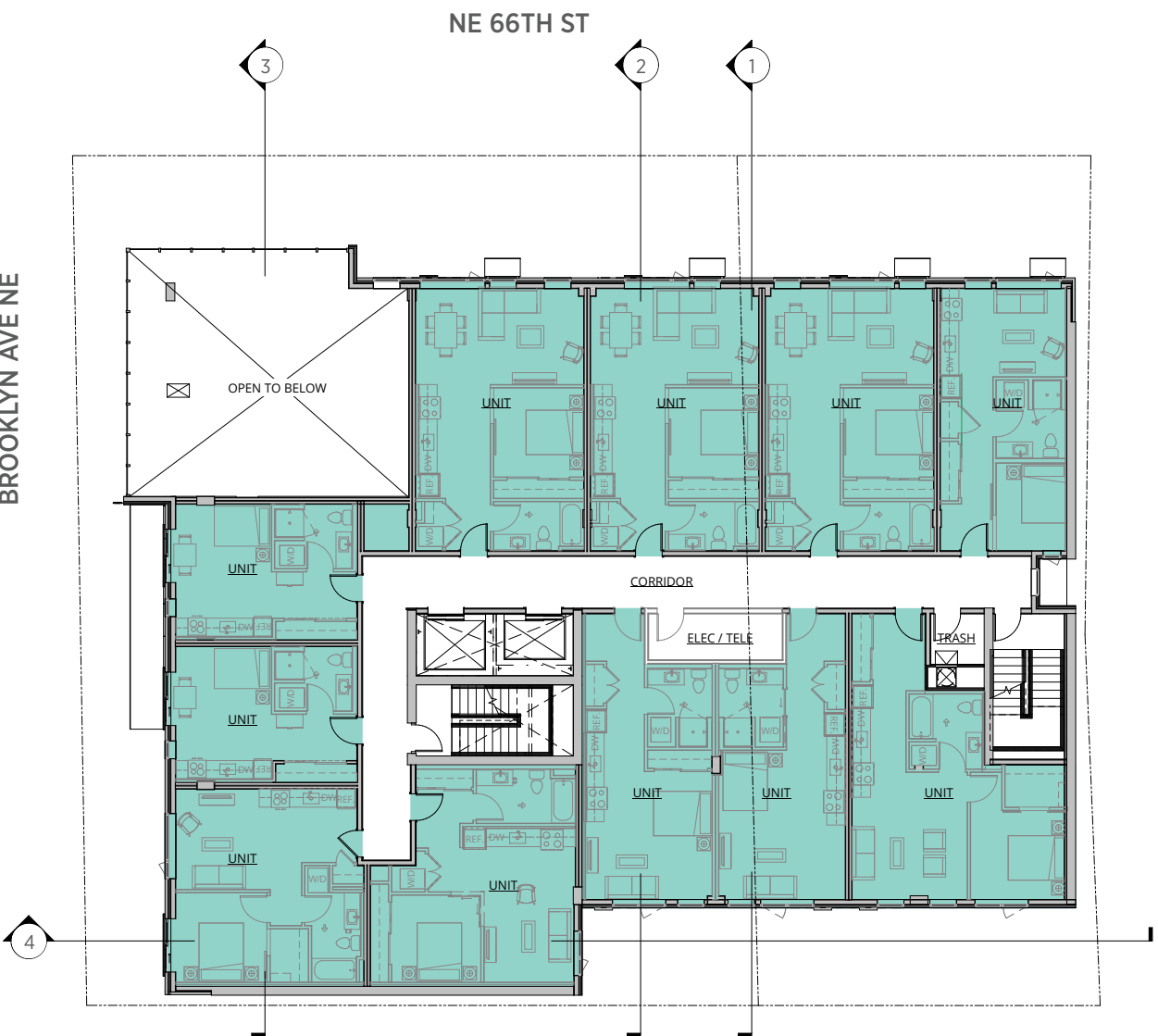
9.0 FLOOR PLANS



LEGEND

- RESIDENTIAL UNITS
- RESIDENTIAL AMENITIES

LEVEL 1 - GROUND



LEVEL 2

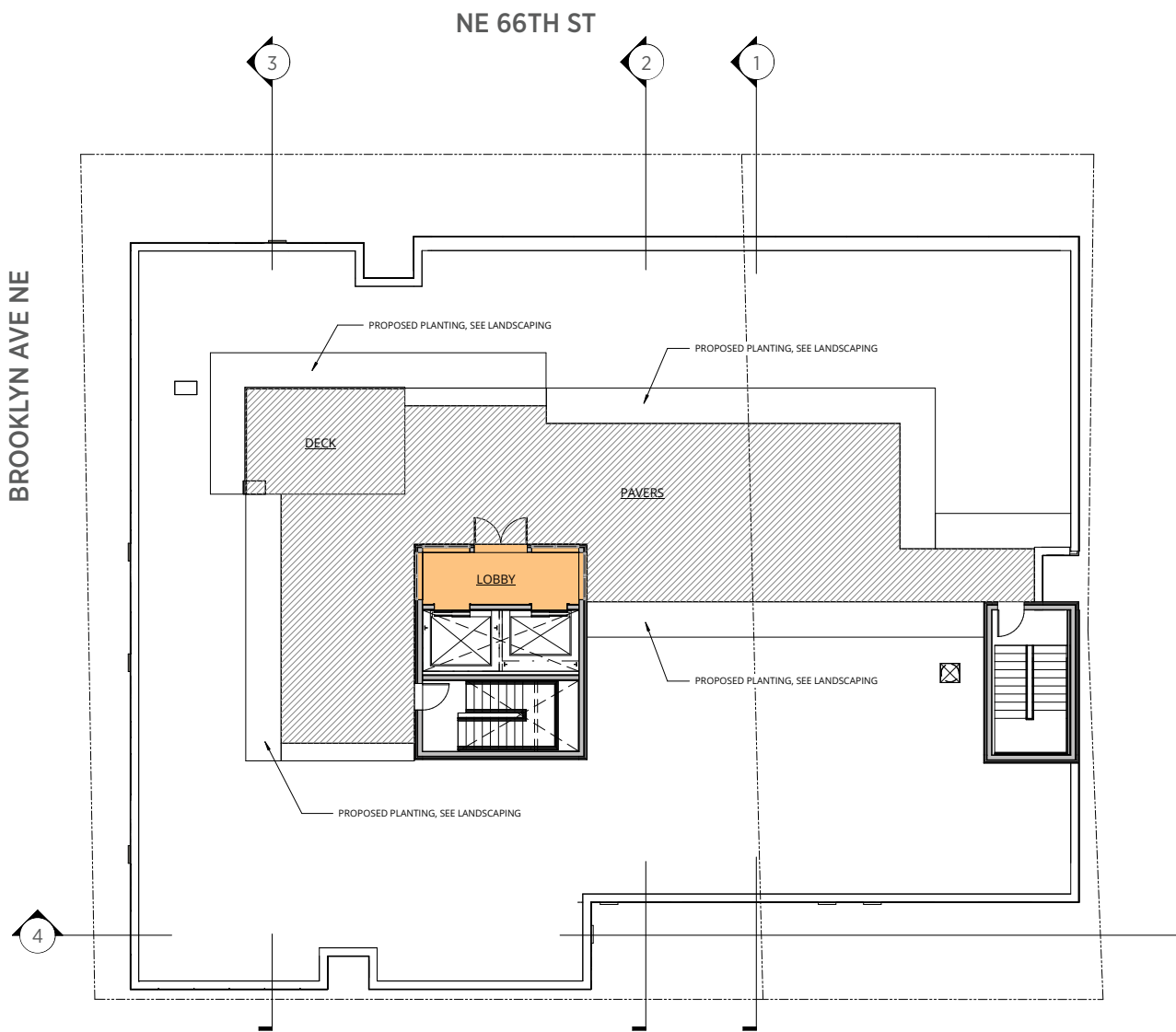




LEGEND

- RESIDENTIAL UNITS
- RESIDENTIAL AMENITIES

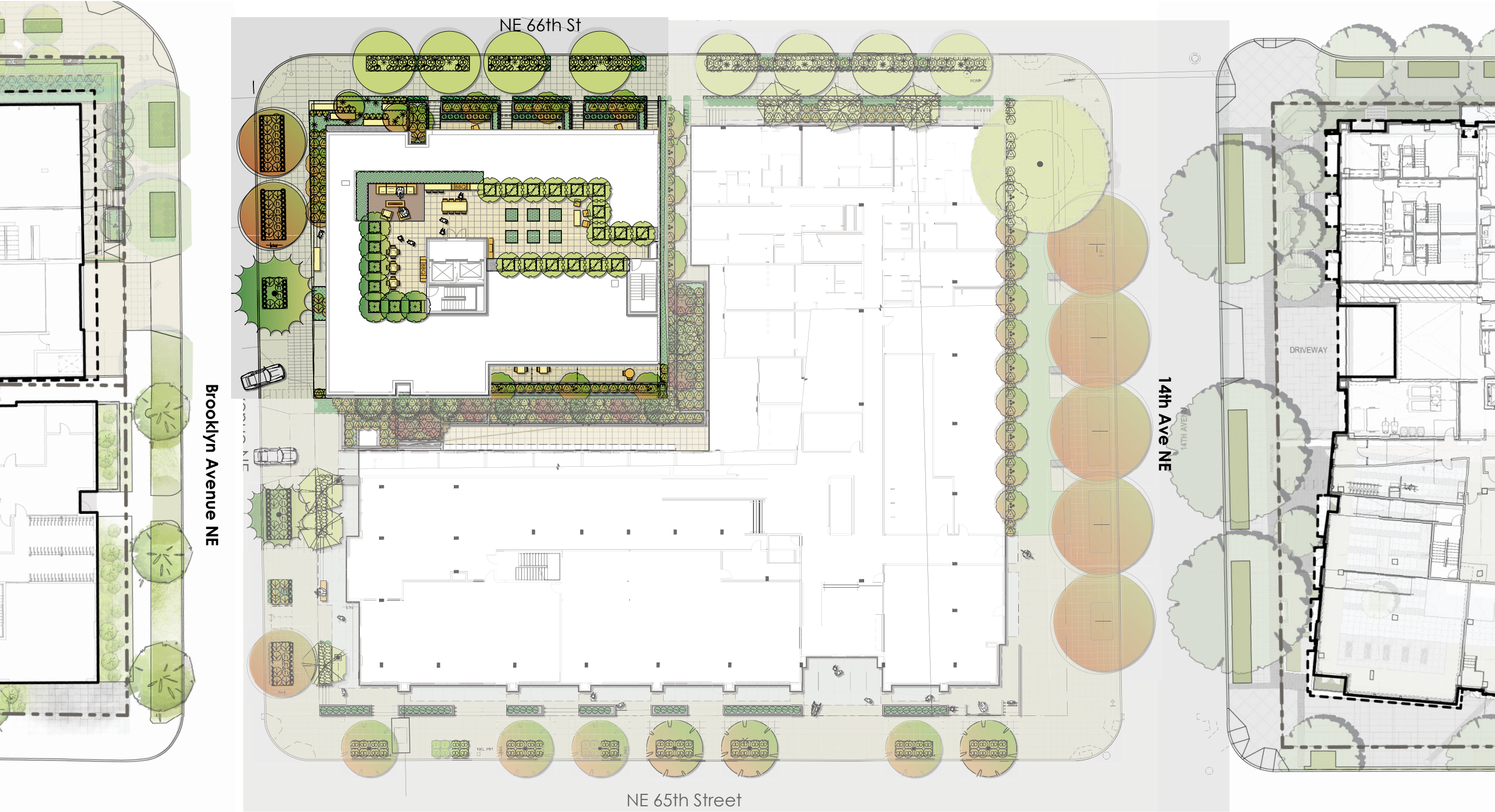
LEVELS 3-7



ROOF LEVEL











corner stone



entry terraces



generous planter w/  
walkoff



bioretention planter





cozy corner



fire and view



outdoor kitchen



treelets





## 10.0 LANDSCAPE / HARDSCAPE PLAN - PLANTS



*Pseudotsuga menziesii*  
Douglas Fir



*Astilbe x arendsii* 'Deutschland'  
'Deutschland' Astilbe



*Helleborus orientalis*  
Lenten Rose



*Pennisetum* 'Little Bunny'  
Little Bunny Dwarf Fountain Grass



*Quercus bicolor* 'JFS-KW12'  
Swamp White Oak



*Berberis buxifolia* 'Nana'  
Dwarf Boxleaf Barberry



*Hydrangea* 'Pee Wee'  
Pee Wee Oakleaf Hydrangea



*Rosa* 'Amber'  
Amber Groundcover Rose



*Cornus kousa* 'Starlight'  
'Starlight' Dogwood



*Buxus microphylla* 'Winter Gem'  
Winter Gem Japanese Boxwood



*Lonicera pileata*  
Boxleaf Hydrangea



*Spiraea* 'Anthony Waterer'  
Anthony Waterer Spirea



*Acer circinatum*  
Vine Maple



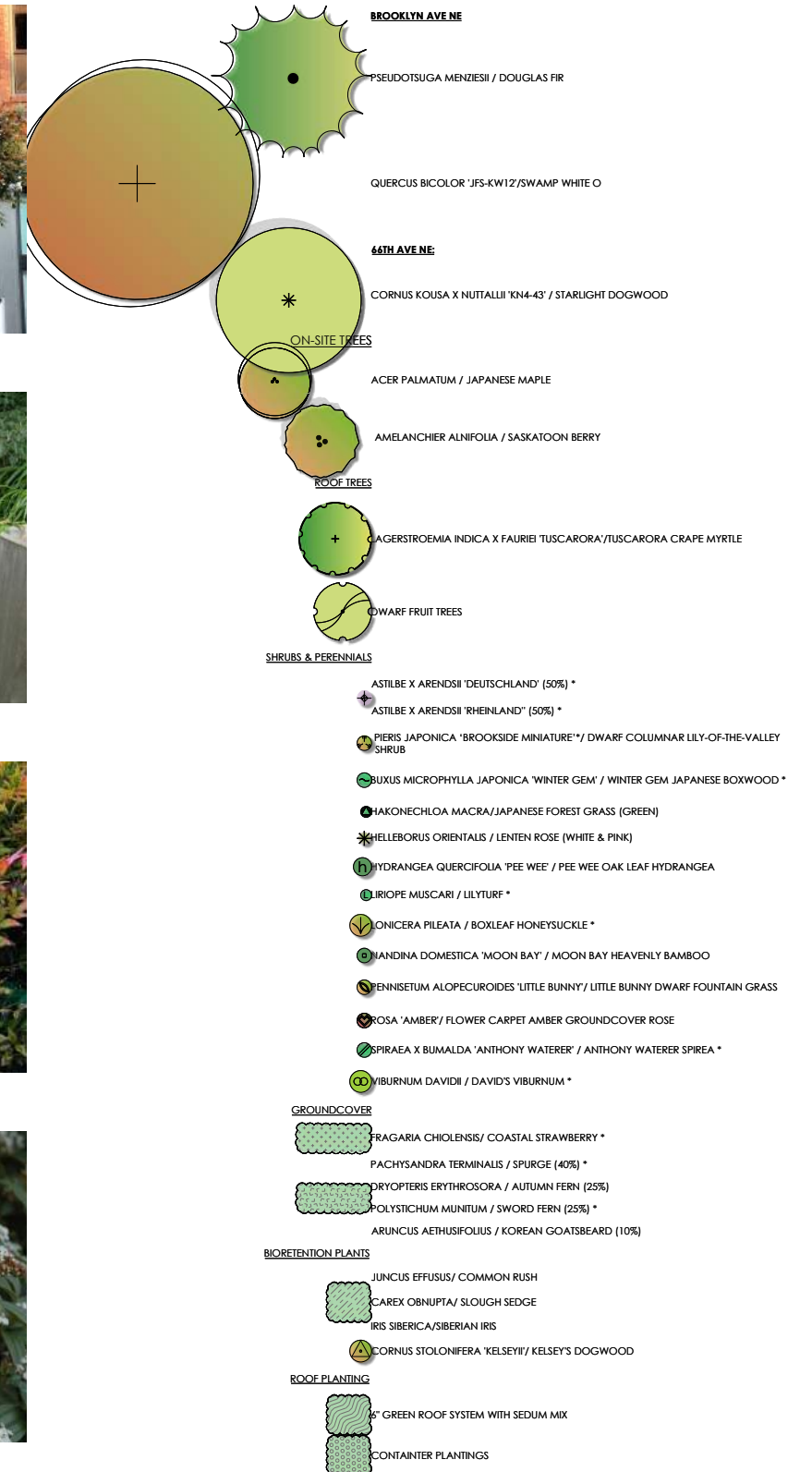
*Hakonechloa macra*  
Hakone species (green)



*Nandina domestica* 'Moon Bay'  
Heavenly Bamboo



*Viburnum davidii*  
David's Viburnum





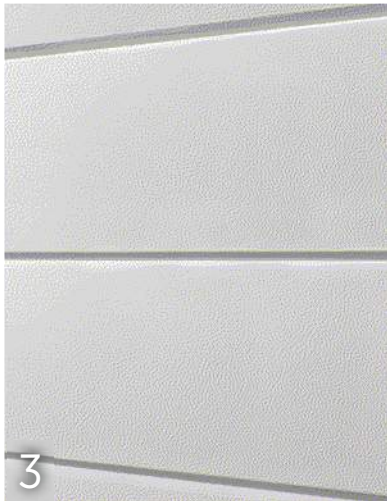




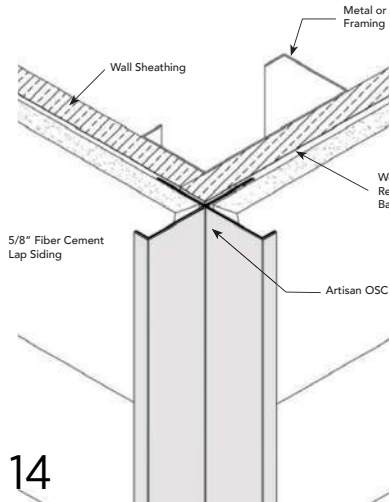




12.0 MATERIAL AND COLOR PALETTE



- LEGEND
- 1. FIBER CEMENT PANEL
  - 2. FIBER CEMENT PANEL
  - 3. FIBER CEMENT V-GROOVE SIDING
  - 4. GLASS RAILING
  - 5. CEDAR TONGUE AND GROOVE
  - 6. BLACK VINYL WINDOW
  - 7. WHITE VINYL WINDOW
  - 8. BLACK STOREFRONT
  - 9. BRAKE METAL
  - 10. DARK BRICK
  - 11. CAST-IN-PLACE CONCRETE
  - 12. PAINTED STEEL
  - 13. METAL MESH RAILING
  - 14. ALUMINUM CORNER TRIM



PAINT 1

PAINT 2

PAINT 3

PAINT 4















14.0 EXTERIOR LIGHTING PLAN

  
  
Ⓐ RECESSED DOWNLIGHT

  
Ⓑ WALL SCENCE

  
Ⓒ LANDSCAPE ACCENT LIGHTING

  
Ⓔ BOLLARD LIGHTING

  
Ⓓ RECESSED PATHWAY LIGHTING

  
Ⓕ EXISTING STREET LAMP







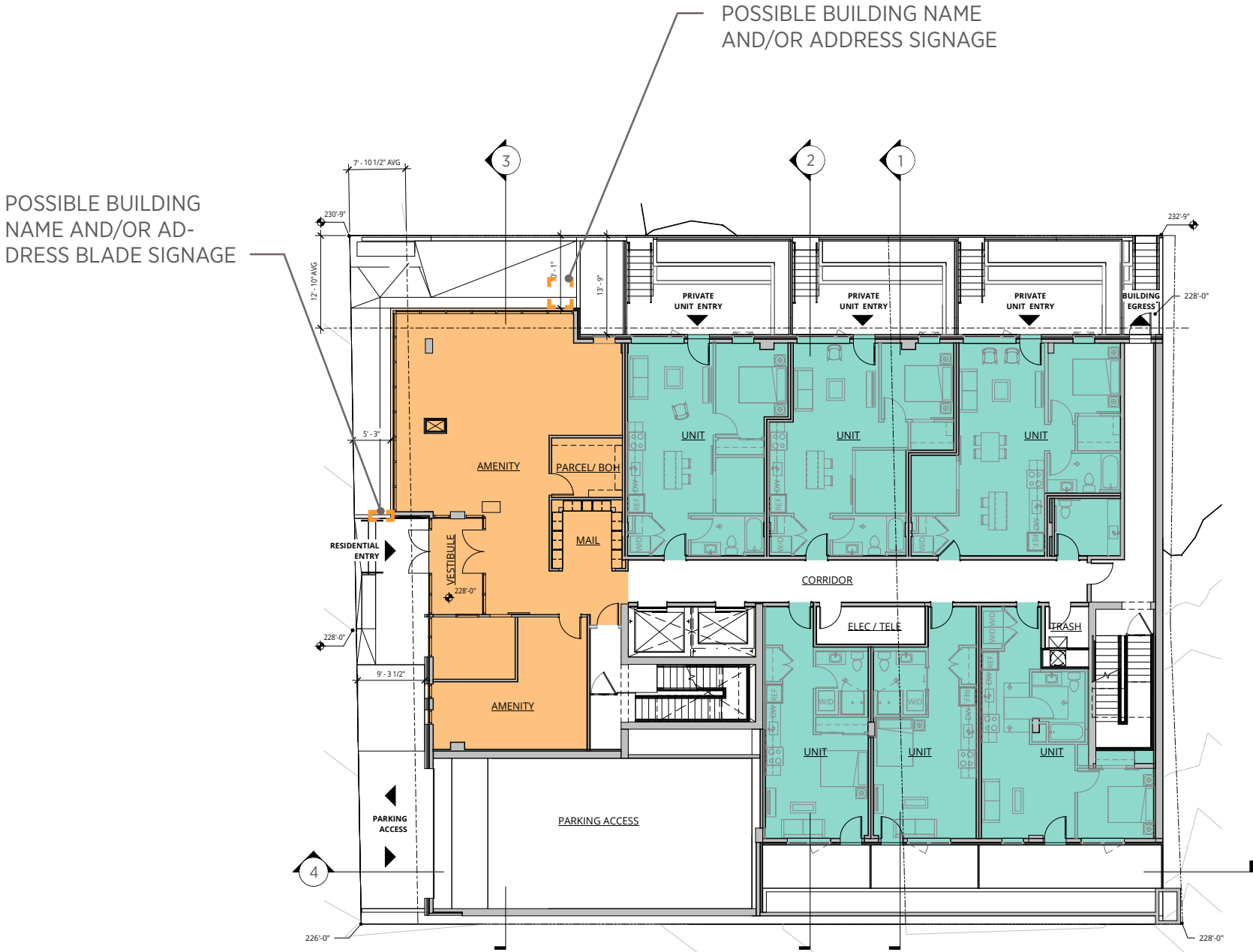
ROOF LIGHTING PLAN 



15.0 SIGNAGE CONCEPT PLAN



SIGNAGE CONCEPT EXAMPLES



GROUND LEVEL SIGNAGE PLAN





POSSIBLE BUILDING NAME AND/OR  
ADDRESS BLADE SIGNAGE

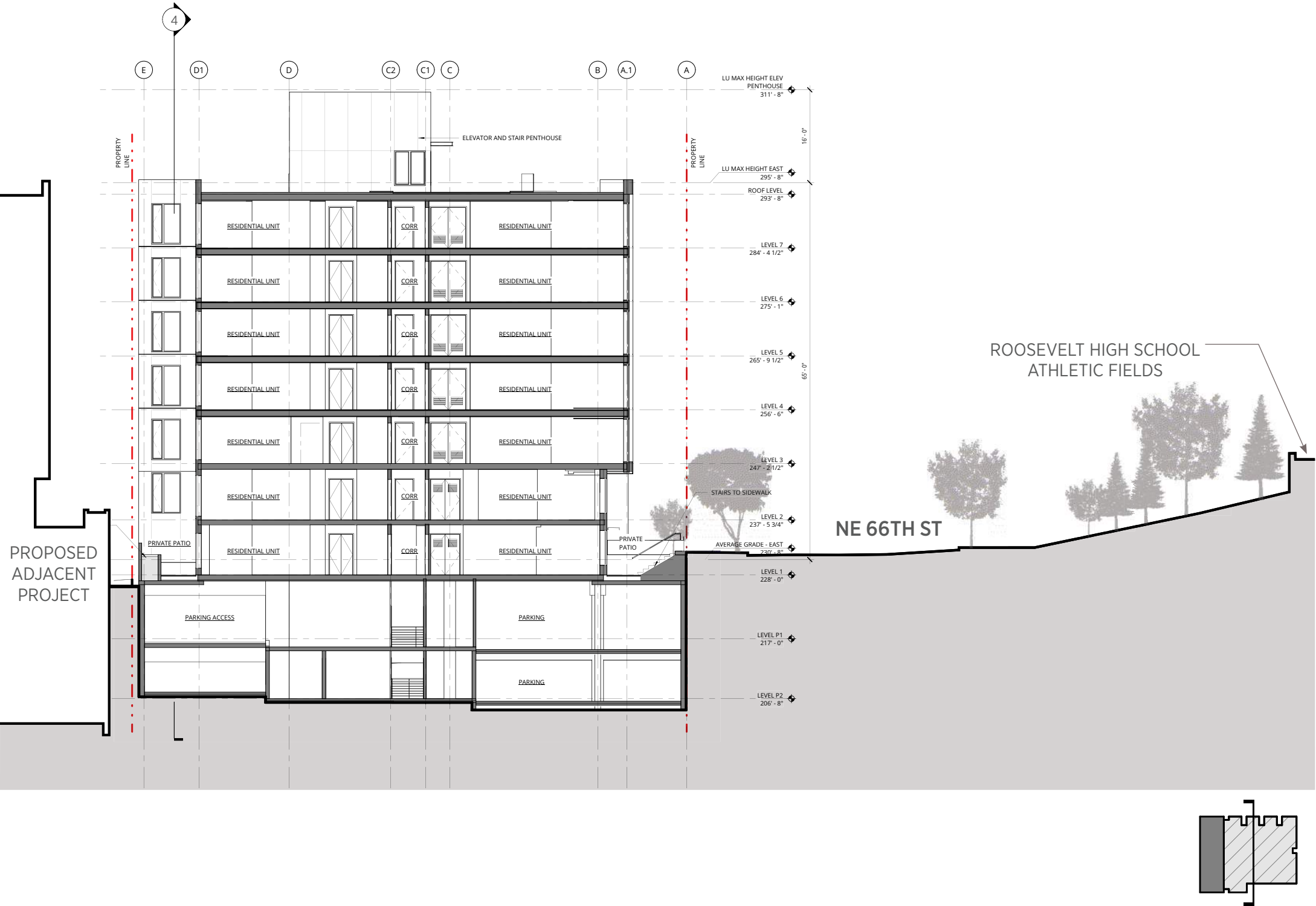
POSSIBLE BUILDING NAME  
AND/OR ADDRESS SIGNAGE

POSSIBLE BLADE BUILDING NAME  
AND/OR ADDRESS SIGNAGE

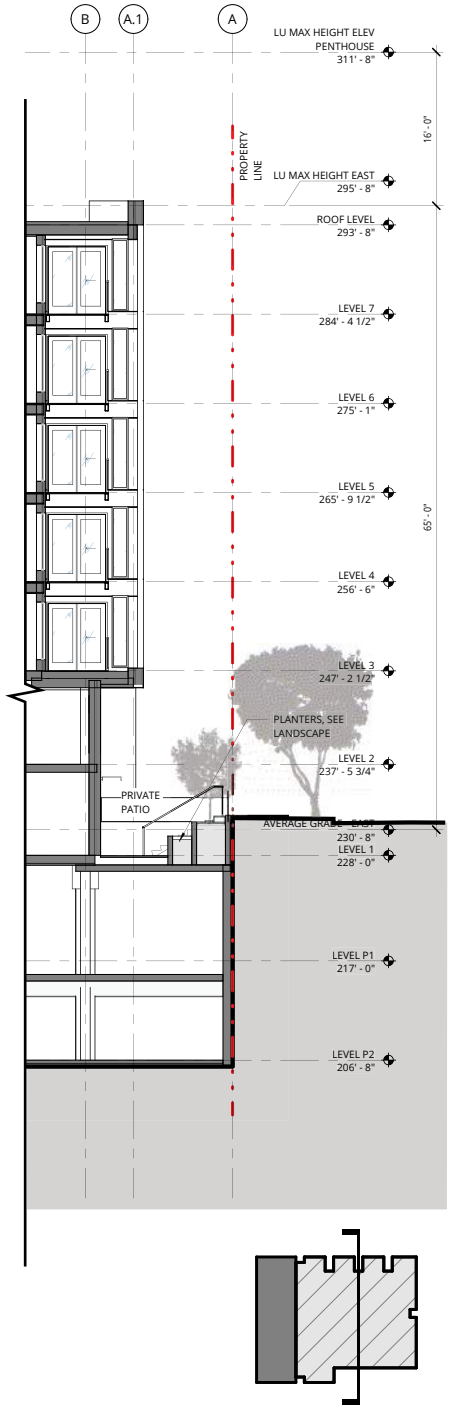




16.0 BUILDING SECTIONS

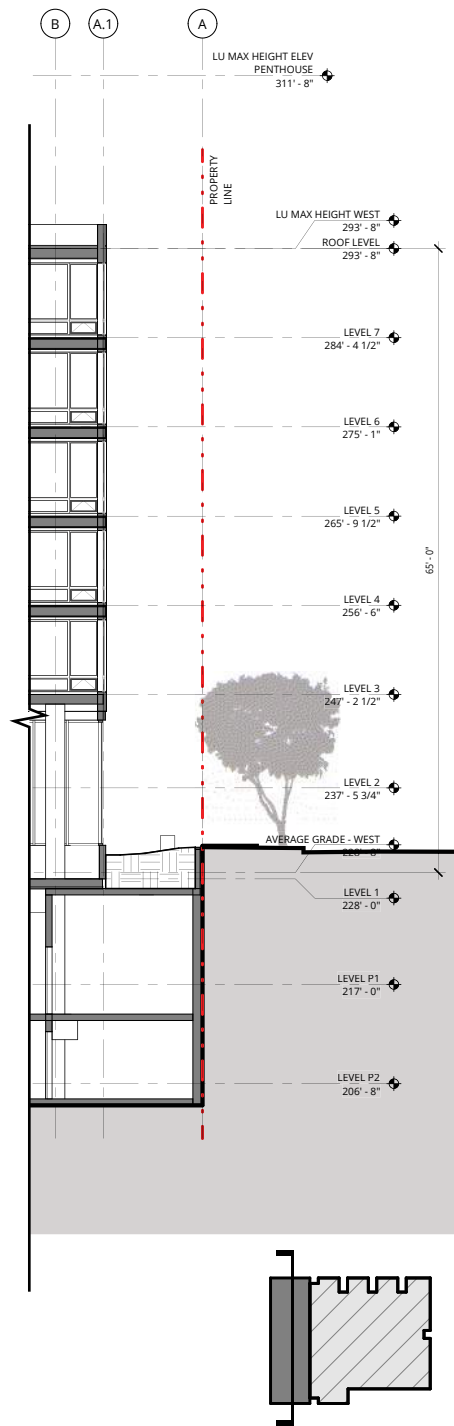


1. N/S BUILDING SECTION

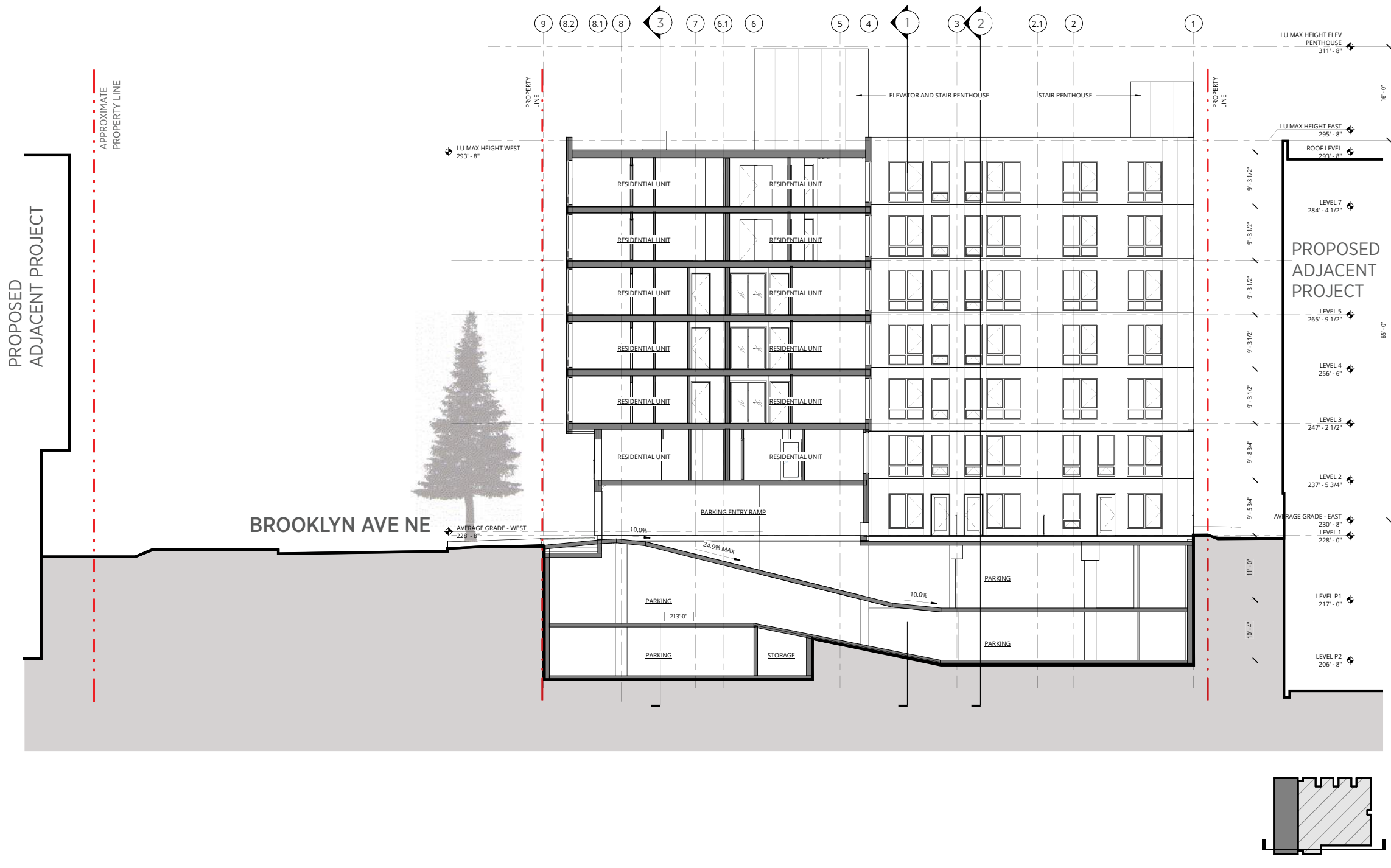


2. N/S BUILDING SECTION AT DECKS





3. N/S BUILDING SECTION AT AMENITY



4. E/W BUILDING SECTION AT DRIVEWAY



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#	CODE:	REQUESTED DEPARTURE:	RATIONALE:	BOARD COMMENT AT EDG:
1	<b>Setback Requirements at NE 66th Street (SMC 23.47A.009.D.1.a.1):</b> The Code requires that along NE 66th St, an average ground-level setback of 10 feet along the length of the street property line and a minimum upper-level setback of 4 feet. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.	Departure from the 4-foot upper level setback and provide an additional setback at the street level and second level instead. The proposed upper level is setback an average of 11'-9" from the property line and the proposed ground-level setback is 13'-9" from the property line.	The departure would allow for more ability to enhance the pedestrian environment along NE 66th St by providing additional space for improvements, including space for street trees, pedestrian seating, and residential unit yards. The departure will better allow the proposed building massing to relate to neighboring proposed and under-construction projects. The code required setback provides imperceptible shading relief on Roosevelt High School and the associated athletic fields.	The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.
2	<b>Street-Level Development Standards –Street Level Street-Facing Facade (SMC 23.47A.008.D.2):</b> The Code requires that street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.	Departure from the maximum 10' distance to the street lot line and instead provide a 13'-9" distance to the street lot line which will include a landscape/open space buffer.	The departure would allow for more ability to enhance the pedestrian environment along NE 66th St by providing additional space for improvements, including space for street trees, pedestrian seating, and residential unit yards. Without an additional 3'-9" setback, the topography of the site would greatly limit potential street activation provided by these patios and would allow less light into the ground level units. Either the depth of the residential patios and/or the landscaping buffer would be reduced therefore diminishing the quality of the patios and planting.	The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.
3	<b>Setback Requirements at Brooklyn Ave NE (SMC 23.47A.009.D.1.a 2):</b> The Code requires that an average ground-level setback of 5 feet along the length of the street property line and a minimum upper-level setback of 4 feet for Brooklyn Avenue NE. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.	Departure from the additional 4-foot upper level setback and instead provide an additional setback at the street level and second level. The proposed upper level is setback an average of 4'-9" from the property line and the proposed ground-level is setback is 9'-3" from the property line.	The departure would allow for more ability to enhance the pedestrian environment along Brooklyn by providing additional space for improvements, including space for street trees and pedestrian seating as well as additional area for garage access site triangle. The code required upper-level setback does not provide views of the school from the street level as recommended in the Roosevelt Neighborhood Design Guidelines. The departure will better allow the proposed building to relate to neighboring proposed and under-construction projects.	The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.
4	<b>Sight Triangle (SMC23.54.030.G.1):</b> The Code requires that for two-way driveways or easements less than 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk. The entrance and exit lanes shall be clearly identified	Departure for a reduction in the required 10 feet unobstructed distance of 10 feet down to 9'-1 3/4".	Use of mirrors and textured pavement in lieu of providing the full sight triangle to diminish presence of garage entry at the street. Brooklyn Ave NE is a side street that experiences an increase in pedestrian traffic during school hours. The reduction in the site triangle depth, as well as using alternative measures will help to minimize any potential impacts to pedestrian traffic, along the street as well as helping to maintain a consistent street frontage. Along with departure 5, a reduced sight triangle allows less area be dedicated to vehicular garage access and increases pedestrian safety by allowing a safer, flat vehicular approach between the sidewalk and the building edge.	The Board indicated preliminary support for this departure request but wanted to see clear documentation as to how and/or why the reduced site triangle works and how the reduced site triangle is safe.



17.0 DEPARTURE #1

CODE:

Setback Requirements at Northeast 66th Street  
(SMC 23.47A.009.D.1.a.1):

The Code requires that along Northeast 66th St, an average ground-level setback of 10 feet along the length of the street property line and a minimum upper-level setback of 4 feet. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.

REQUESTED DEPARTURE:

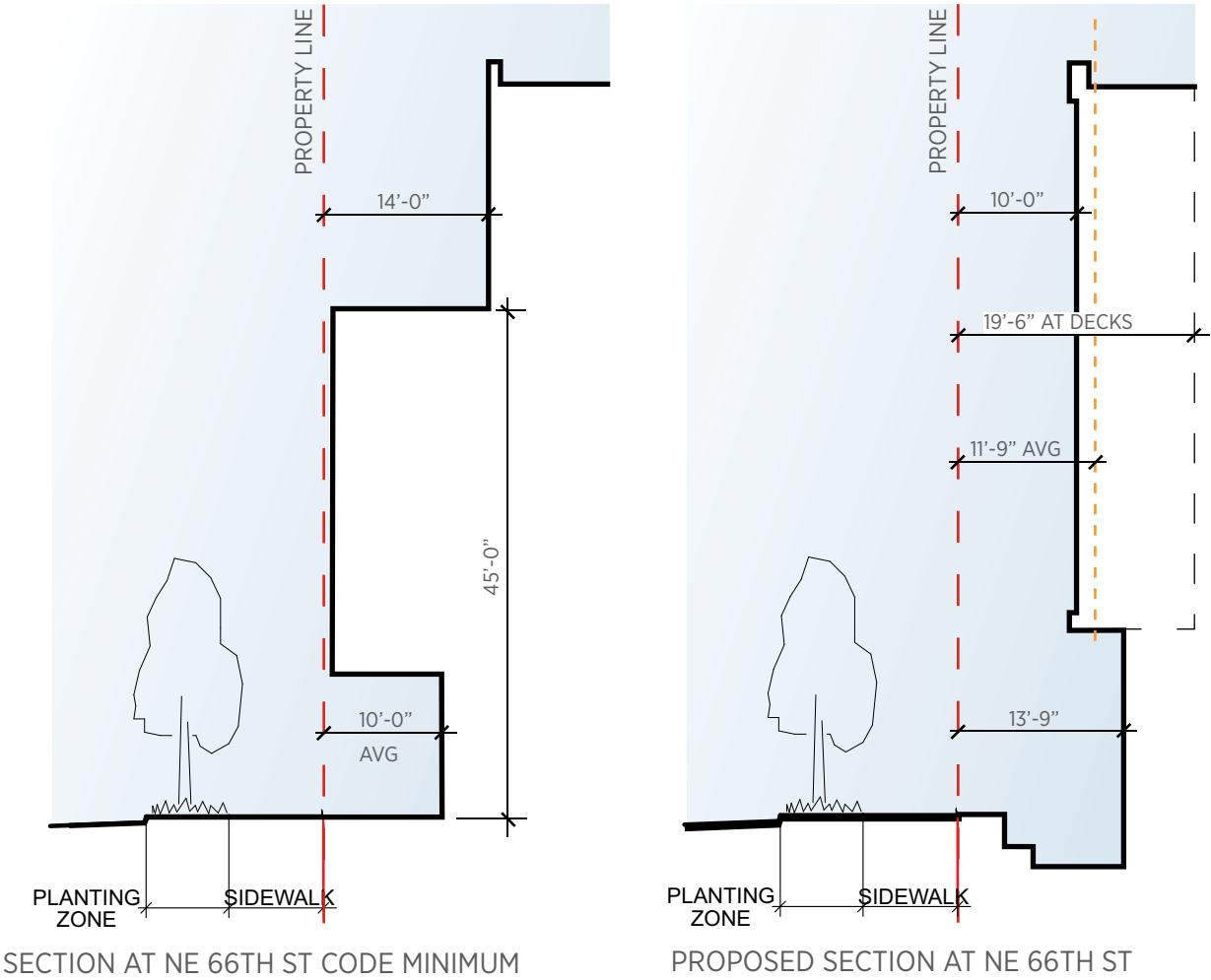
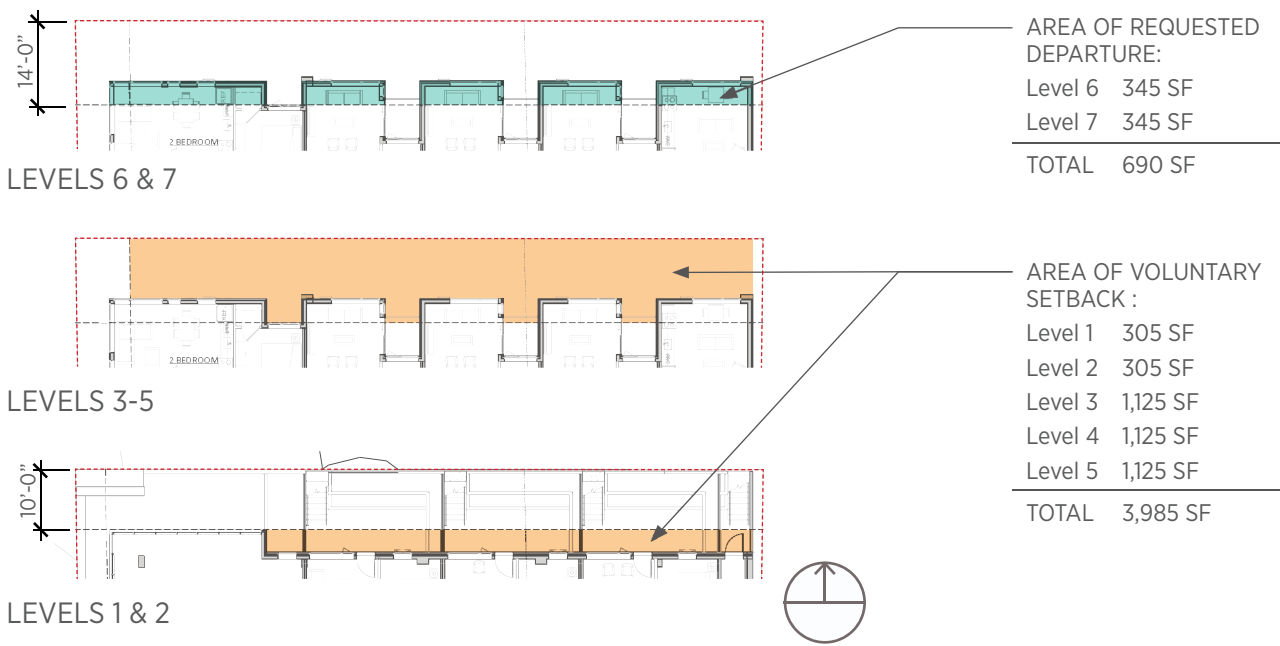
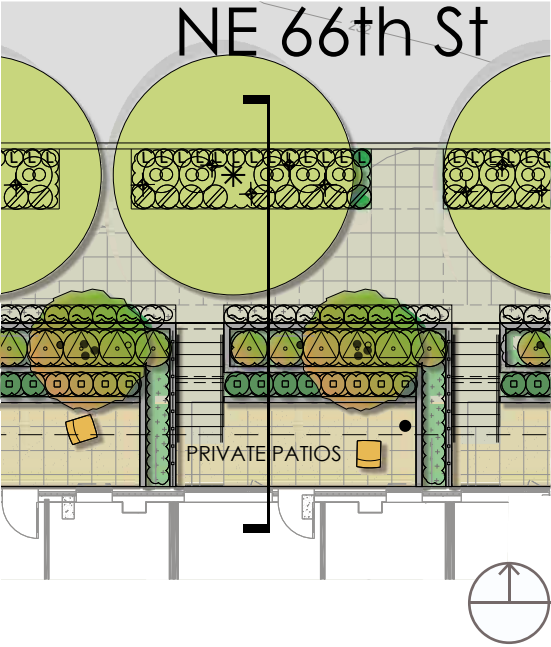
Departure from the 4-foot upper level setback and provide an additional setback at the street level and second level instead. The proposed upper level is setback an average of 11'-9" from the property line and the proposed ground-level setback is 13'-9" from the property line.

RATIONALE:

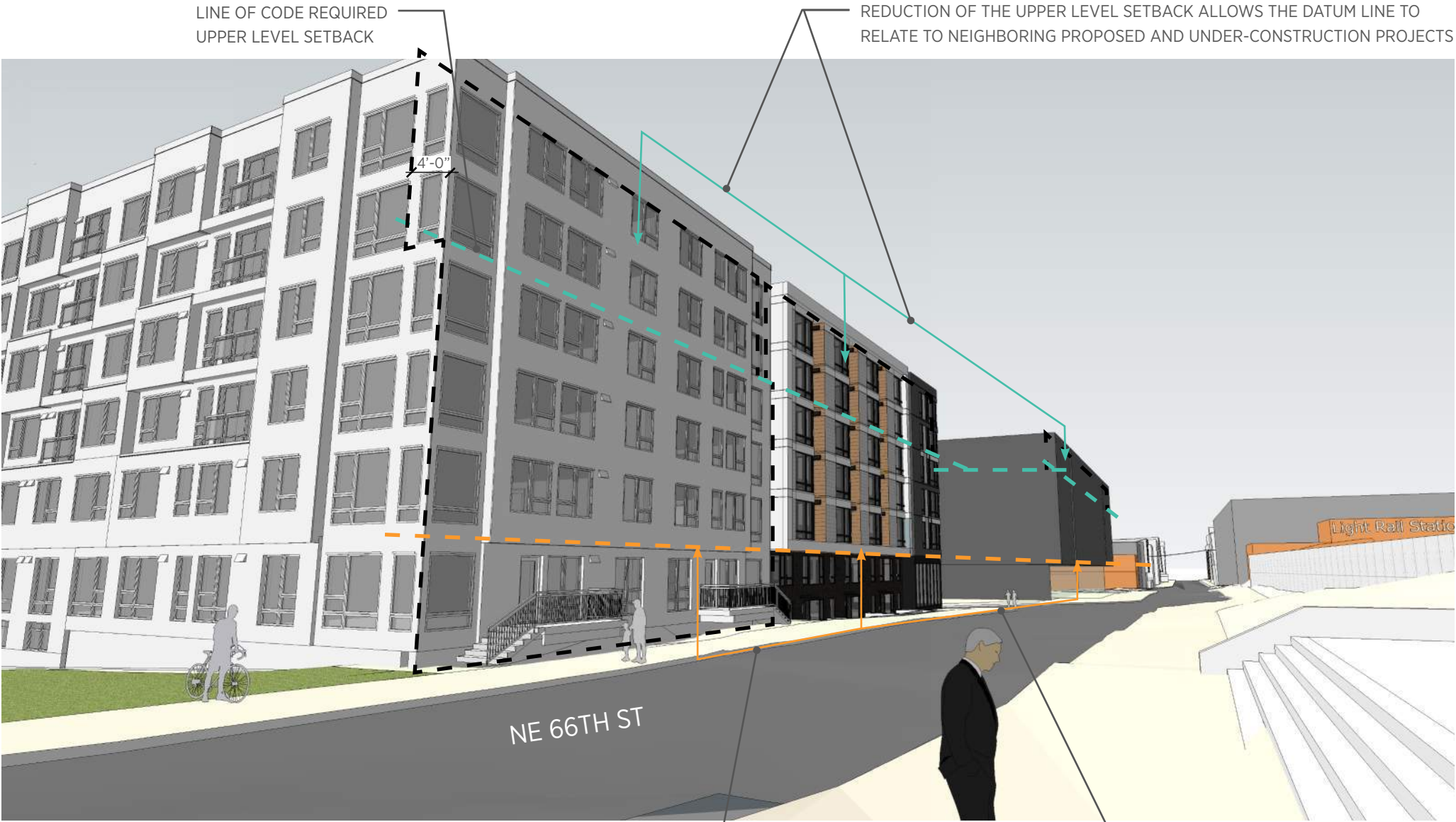
The departure would allow for more ability to enhance the pedestrian environment along NE 66th St by providing additional space for improvements, including space for street trees, pedestrian seating, and residential unit yards. The departure will better allow the proposed building massing to relate to neighboring proposed and under-construction projects. The code required setback provides imperceptible shading relief on Roosevelt High School and the associated athletic fields.

BOARD COMMENT AT EDG:

The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.







VIEW WEST ALONG NE 66TH ST

A REDUCTION OF THE UPPER LEVEL SETBACK ALLOWS FOR THE DATUM LINE OF THE BUILDING BASE (GROUND LEVEL SETBACK) TO BE RAISED TO THE UNDERSIDE OF LEVEL 3, THEREFORE ALIGN WITH THE NEIGHBORING BUILDING.



VIEW EAST ALONG NE 66TH ST



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SUN STUDY ON SEPTEMBER 21 AT 3:00 PM - ZONING MASSING



SUN STUDY ON DECEMBER 21 AT 3:00 PM - ZONING MASSING



SUN STUDY ON SEPTEMBER 21 AT 3:00 PM - PROPOSED MASSING



SUN STUDY ON DECEMBER 21 AT 3:00 PM - PROPOSED MASSING



17.0 DEPARTURE #2

CODE:

Street-Level Development Standards –Street Level Street-Facing Facade (SMC 23.47A.008.D.2):

The Code requires that street-level street-facing facades shall be located within 10 feet of the street lot line, unless wider sidewalks, plazas, or other approved landscaped or open spaces are provided.

REQUESTED DEPARTURE:

Departure from the maximum 10’ distance to the street lot line and instead provide a 13’-9” distance to the street lot line which will include a landscape/open space buffer.

RATIONALE:

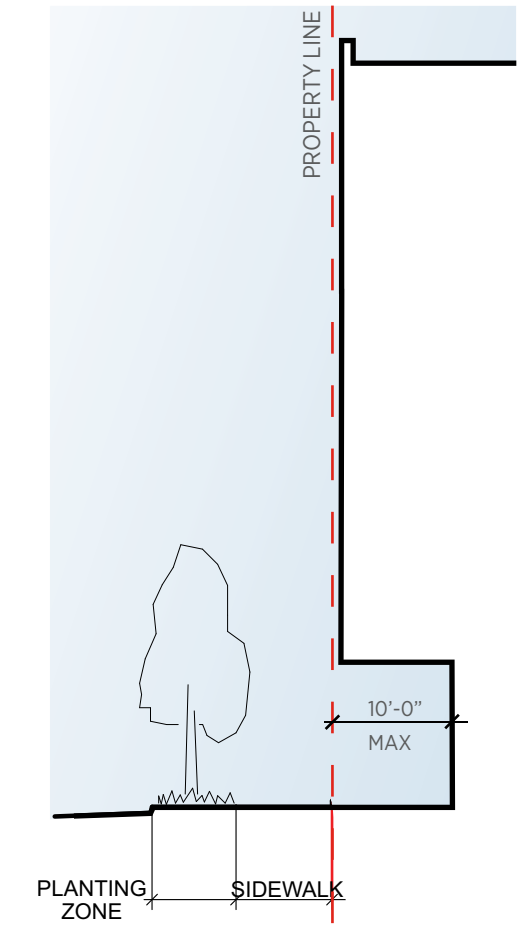
The departure would allow for more ability to enhance the pedestrian environment along NE 66th St by providing additional space for improvements, including space for street trees, pedestrian seating, and residential unit yards. Without an additional 3’-9” setback, the topography of the site would greatly limit potential street activation provided by these patios and would allow less light into the ground level units. Either the depth of the residential patios and/or the landscaping buffer would be reduced therefore diminishing the quality of the patios and planting.

BOARD COMMENT AT EDG:

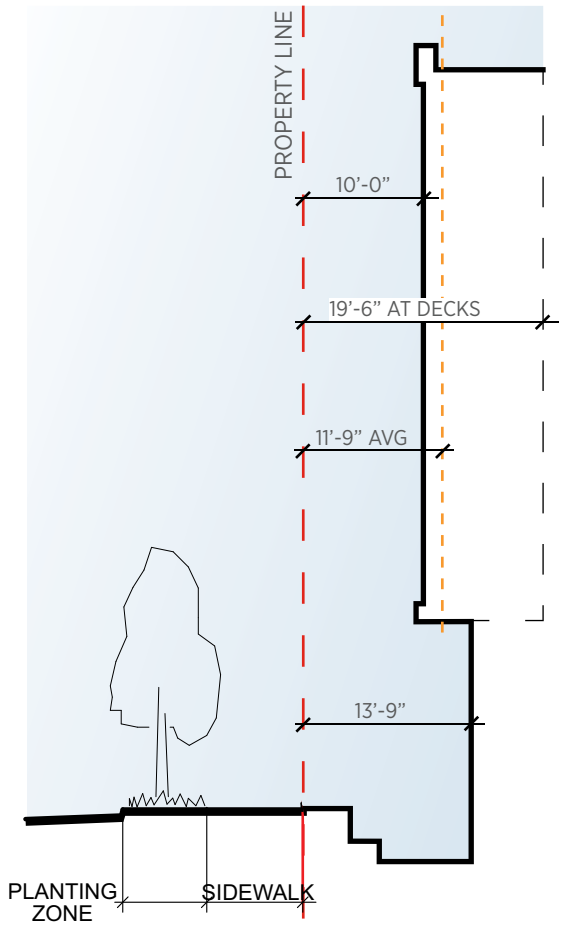
The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.



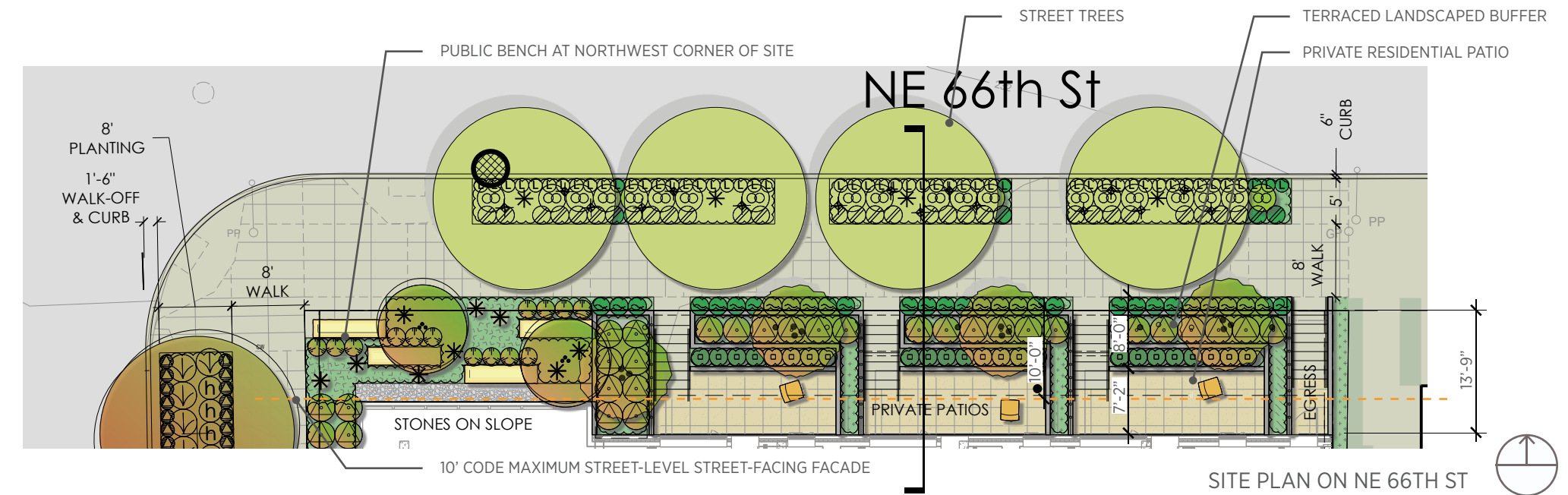
NEIGHBORHOOD EXAMPLE: PROPOSED STREETScape ON NE 66TH ST



SECTION AT NE 66TH ST CODE MINIMUM



PROPOSED SECTION AT NE 66TH ST







PROPOSED STREETScape ON NE 66TH ST



17.0 DEPARTURE #3

CODE:

Setback Requirements at Brooklyn Ave NE  
(SMC 23.47A.009.D.1.a 2):

The Code requires that an average ground-level setback of 5 feet along the length of the street property line and a minimum upper-level setback of 4 feet for Brooklyn Avenue NE. The minimum upper-level setback shall be provided in addition to the required ground-level setback at all points along the length of the street property line at 45 feet of height and above, as measured from average finished grade.

REQUESTED DEPARTURE:

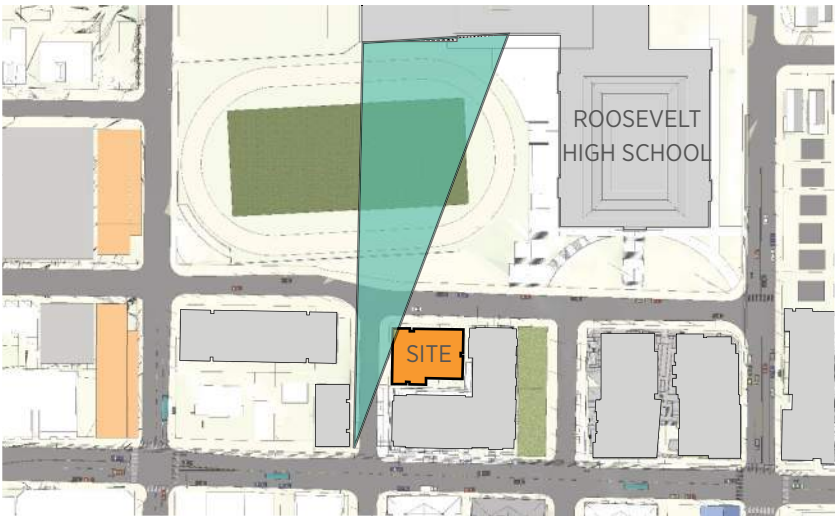
Departure from the additional 4-foot upper level setback and instead provide an additional setback at the street level and second level. The proposed upper level is setback an average of 4'-9" from the property line and the proposed ground-level is setback is 9'-3" from the property line.

RATIONALE:

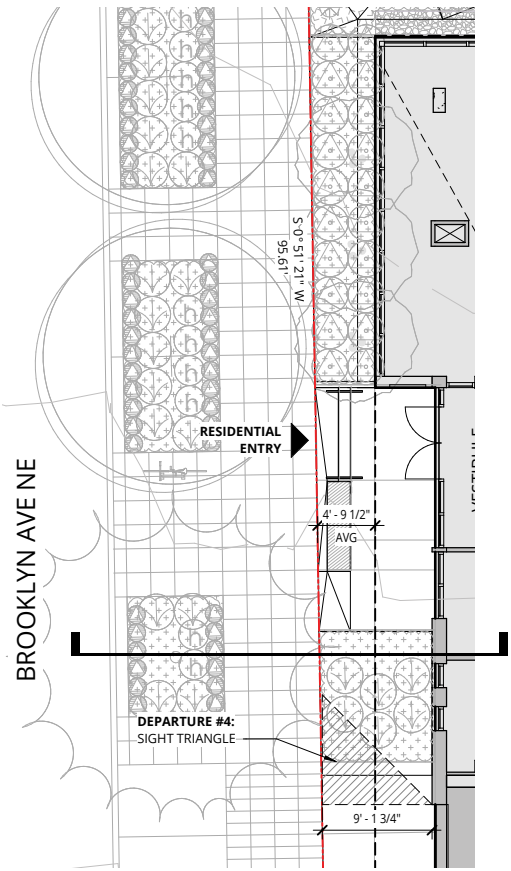
The departure would allow for more ability to enhance the pedestrian environment along Brooklyn by providing additional space for improvements, including space for street trees and pedestrian seating as well as additional area for garage access site triangle. The code required upper-level setback does not provide views of the school from the street level as recommended in the Roosevelt Neighborhood Design Guidelines. The departure will better allow the proposed building to relate to neighboring proposed and under-construction projects.

BOARD COMMENT AT EDG:

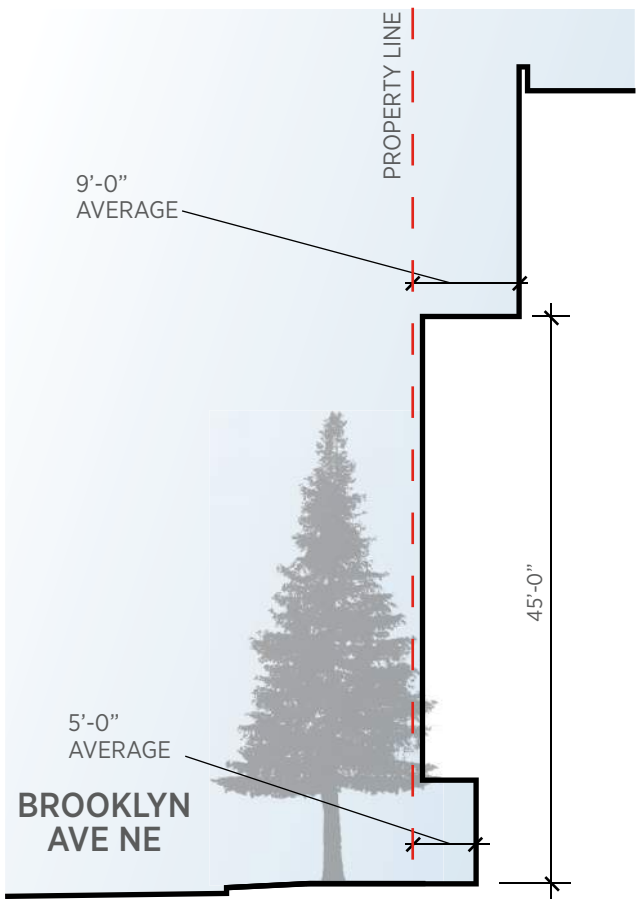
The Board indicated preliminary support for this departure request provided the street level experience is truly enhanced by the highlighted pedestrian amenities and high quality landscape design.



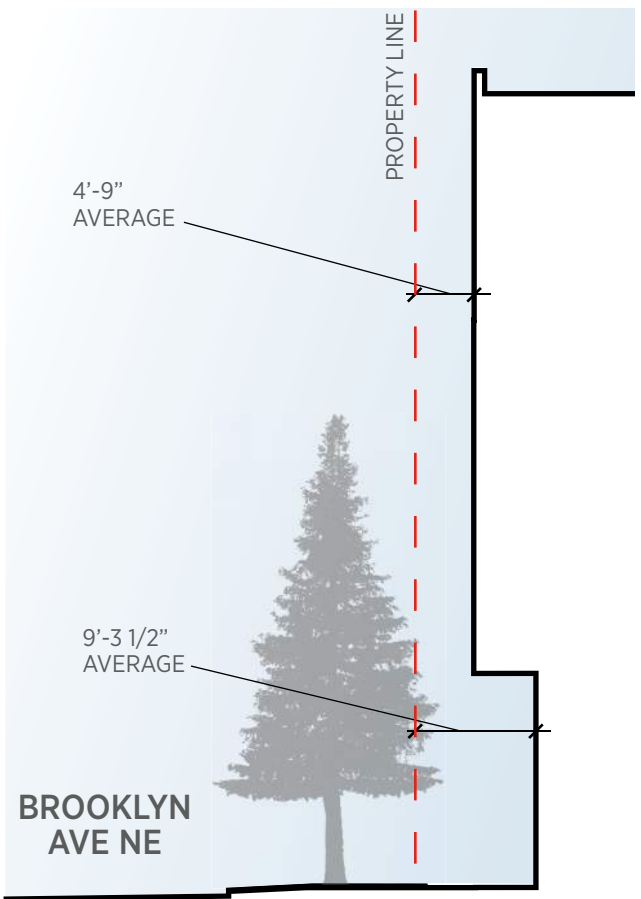
VICINITY PLAN WITH HIGH SCHOOL VIEW TRIANGLE  
PROPOSED PROJECT HAS LIMITED IMPACT ON THE VIEW  
OF ROOSEVELT HIGH SCHOOL



SITE PLAN ON BROOKLYN AVE NE



CODE REQUIRED SECTION ON BROOKLYN AVE NE



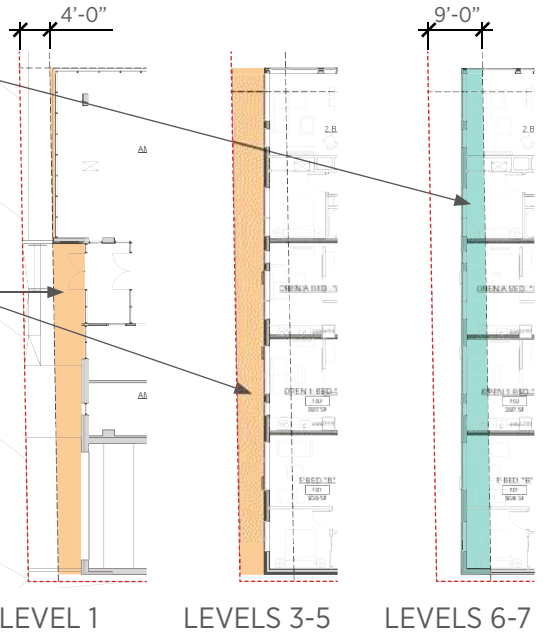
PROPOSED SECTION ON BROOKLYN AVE NE

AREA OF REQUESTED DEPARTURE:

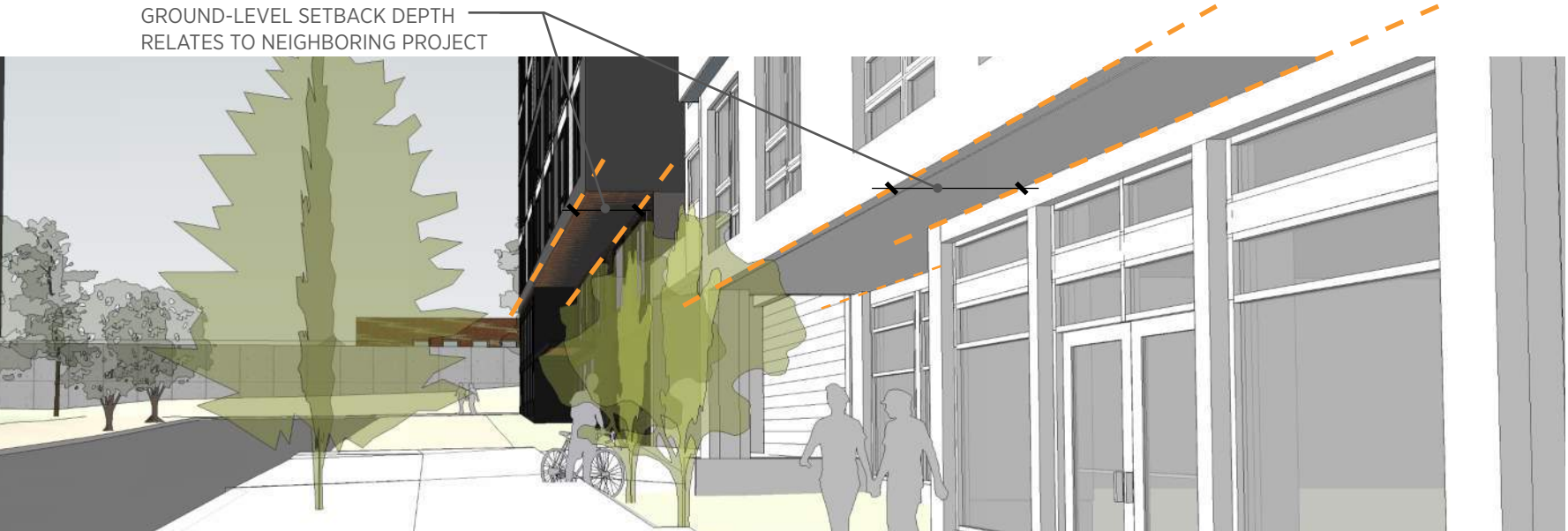
Level 6	360 SF
Level 7	360 SF
TOTAL	720 SF

AREA OF VOLUNTARY SETBACK:

Level 1	265 SF
Level 2	265 SF
Level 3	400 SF
Level 4	400 SF
Level 5	400 SF
TOTAL	1,730 SF







VIEW NORTH ON SIDEWALK ALONG BROOKLYN AVE NE



VIEW FROM NE 65TH ST AND BROOKLYN AVE NE - PROPOSED MASSING



VIEW NORTH ON SIDEWALK ALONG BROOKLYN AVE NE



17.0 DEPARTURE #4

CODE:

Sight Triangle (SMC23.54.030.G.1):

The Code requires that for two-way driveways or easements 22 feet wide or more, a sight triangle on the side of the driveway used as an exit shall be provided, and shall be kept clear of any obstruction for a distance of 10 feet from the intersection of the driveway or easement with a driveway, easement, sidewalk, or curb intersection if there is no sidewalk. The entrance and exit lanes shall be clearly identified.

REQUESTED DEPARTURE:

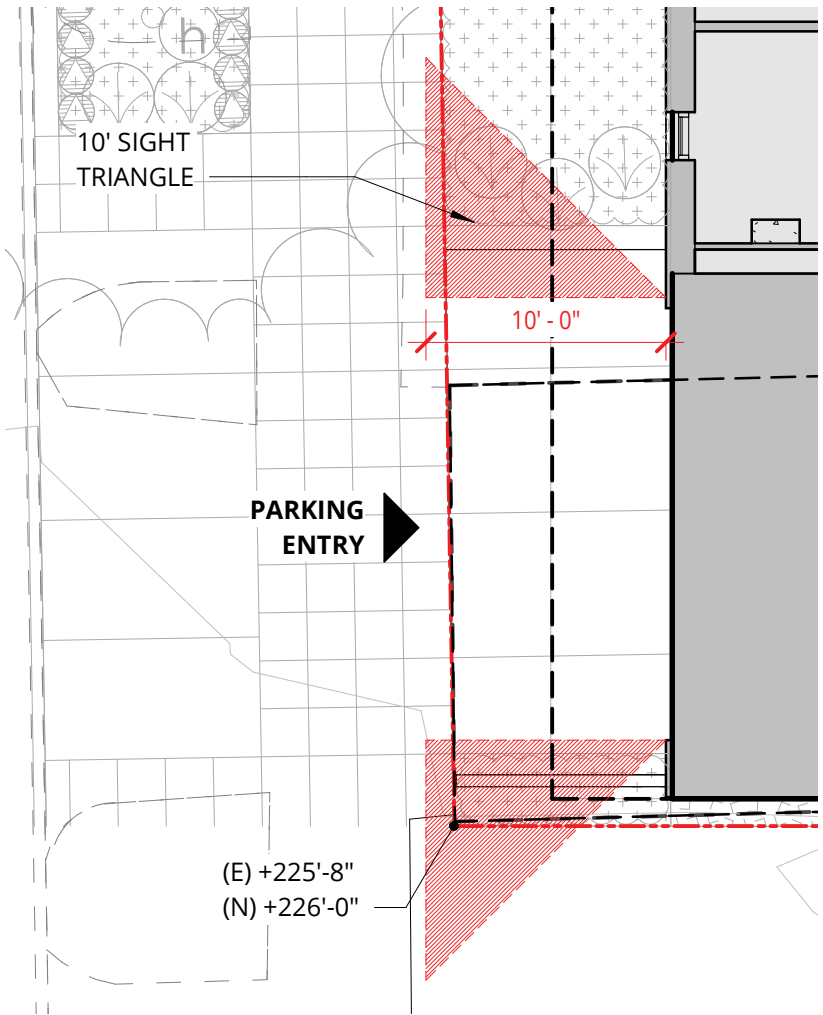
Departure for a reduction in the required 10 feet unobstructed distance of 10 feet down to 9'-1 3/4".

RATIONALE:

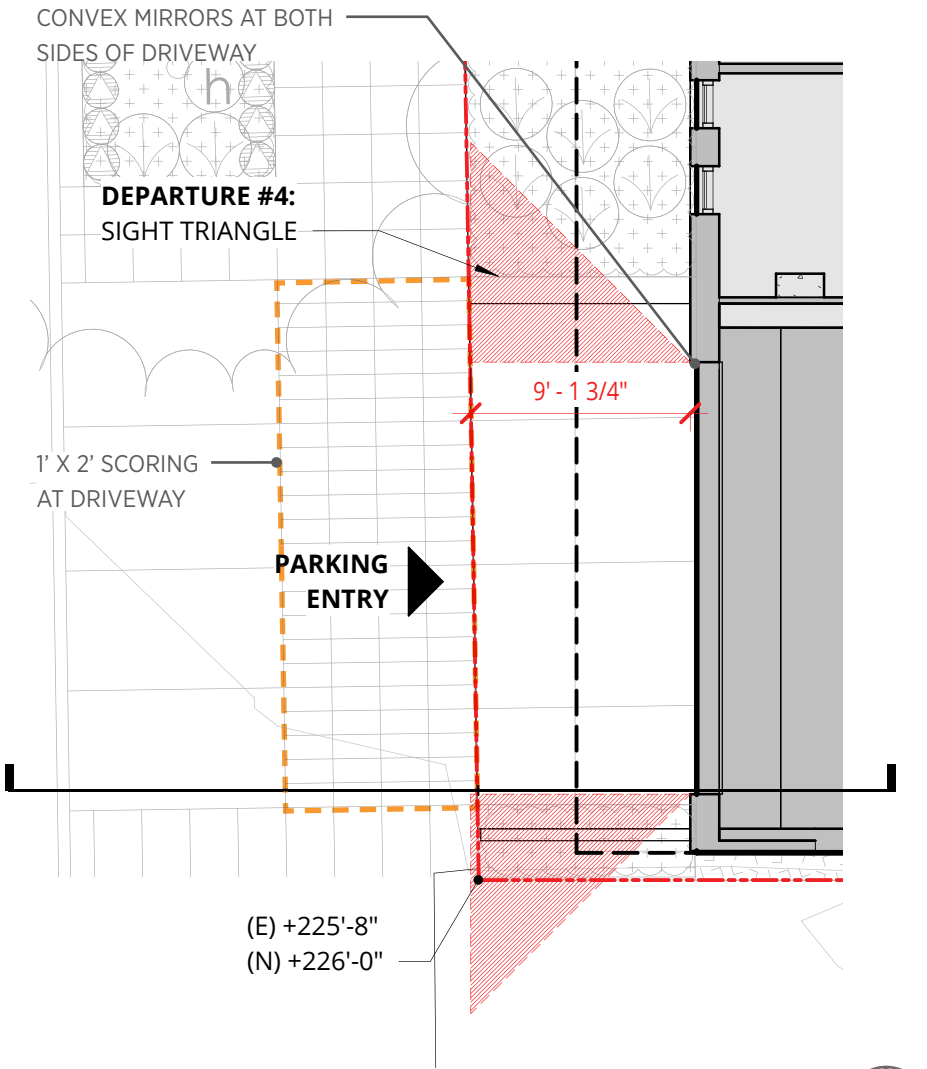
Use of mirrors and textured pavement in lieu of providing the full sight triangle to diminish presence of garage entry at the street. Brooklyn Ave NE is a side street that experiences an increase in pedestrian traffic during school hours. The reduction in the site triangle depth, as well as using alternative measures will help to minimize any potential impacts to pedestrian traffic, along the street as well as helping to maintain a consistent street frontage. Along with departure 5, a reduced sight triangle allows less area be dedicated to vehicular garage access and increases pedestrian safety by allowing a safer, flat vehicular approach between the sidewalk and the building edge.

BOARD COMMENT AT EDG:

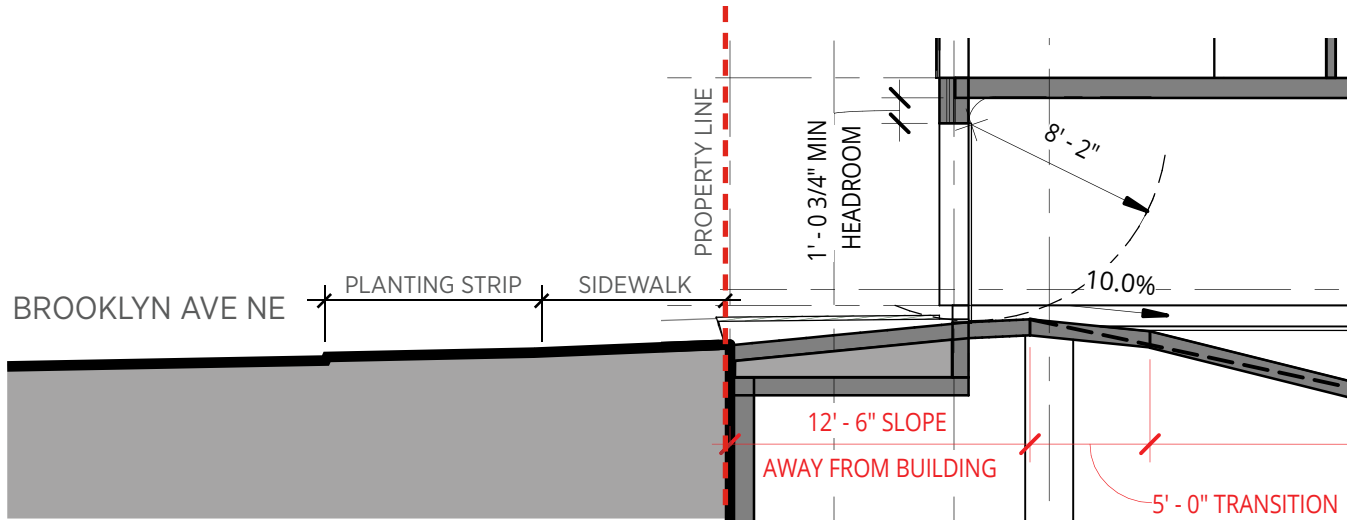
The Board indicated preliminary support for this departure request but wanted to see clear documentation as to how and/or why the reduced site triangle works and how the reduced site triangle is safe.



SITE PLAN WITH FULL SIGHT TRIANGLE AT PARKING ENTRY



PROPOSED SITE PLAN AT PARKING ENTRY WITH DEPARTURE



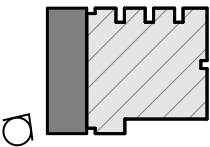
PROPOSED DRIVEWAY SECTION





EXAMPLE OF MIRROR

1' X 2' SCORE PATTERN AT DRIVEWAY





APPENDIX - EXISTING TREES

SIGNIFICANT TREES

EXISTING TREE TO BE REMOVED:  
REFER BELOW FOR IDENTIFICATION & SIZE

TREE #	SPECIES	COMMON NAME	DBH	DRIP LINE	CURRENT HEALTH RATING	EXCEPTIONAL	STATUS
2	Ilex aquifolium	English Holly	7.1"	10'	Fair	-	Remove
3	Prunus sp.	Fruiting Cherry	10.4"	16'	Good	-	Remove
4	Cornus florida	Pink Flowering Dogwood	5.2"	6'	Poor	-	Remove
5	Thuja plicata	Western Red Cedar	28.1"	24'	Very good	-	Remove
6	Pyrus sp.	Pear	5.8"	14'	Poor	-	Remove
7	Prunus sp.	Fruiting Cherry	16.6"	20'	Fair	-	Remove
8	Prunus sp.	Fruiting Cherry	13.6"	16'	Fair	-	Remove
9	Ulmus americana	American Elm	15.9"	22'	Fair	-	Remove
10	Prunus sp.	Flowering Cherry	11.8"	12'	Fair	-	Remove
11	Malus sp.	Apple	10.6"	16'	Dying	-	Remove
12	Thuja plicata	Western Red Cedar	9.8"	13'	Good	-	Remove
31	Ilex aquifolium	English Holly	12.9"	14'	Fair	-	Remove
ROW Trees*							
A	Prunus laurocerasus	English Laurel	15.5"	12'	Good	-	Remove
Off-site Trees (to be removed under separate permit)							
B*	Prunus 'Kwanzan'	Kwanzan Flowering Cherry	15.3"	16'	Fair	-	Remove
13	Chamaecyparis pisifera	Sawara Cypress	21.6"	16'	Fair	-	Remove
17	Sciadopitys verticillata	Japanese Umbrella Pine	9.6"	12'	Good	-	Remove
24	Prunus sp.	Fruiting Cherry	14.2"	12'	Fair	-	Remove

NOTE:

Tree identification, diameter at breast height (dbh), drip line, and health by Brian K. Gilles, Gilles Consulting, 425-822-4944, as shown in report dated December 13, 2017.

\*Trees in right-of-way, identification based on SDOT Street Tree Inventory. Street Trees are approved for removal, per email with Bill Ames with SDOT Urban Forestry, 3/17/2017. Removal requires a no-fee SDOT Urban Forestry permit, and standard 14 day public notification prior to being removed. Coordinate ROW tree removal permit directly with Ben Roberts ben.roberts@seattle.gov ph: 206.233.8735

SIGNIFICANT & EXCEPTIONAL TREE NOTES

1. 12 SIGNIFICANT TREES ARE ON THE PROPERTY & 1 IN THE RIGHT-OF-WAY. 4 TREES ON ADJACENT PROPERTIES OVERHANG THIS PROPERTY.
2. ALL SIGNIFICANT TREES WILL BE REMOVED.

TREE REPLACEMENT NOTES

1. PER SMC 25.11.09:

A. EACH EXCEPTIONAL TREE AND TREES OVER TWO (2) FEET IN DIAMETER THAT IS REMOVED IN ASSOCIATION WITH DEVELOPMENT IN ALL ZONES SHALL BE REPLACED BY ONE OR MORE NEW TREES, THE SIZE AND SPECIES OF WHICH SHALL BE DETERMINED BY THE DIRECTOR; THE TREE REPLACEMENT REQUIRED SHALL BE DESIGNED TO RESULT, UPON MATURITY, IN A CANOPY COVER THAT IS AT LEAST EQUAL TO THE CANOPY COVER PRIOR TO TREE REMOVAL. PREFERENCE SHALL BE GIVEN TO ON-SITE REPLACEMENT, WHEN ON-SITE REPLACEMENT CANNOT BE ACHIEVED, OR IS NOT APPROPRIATE AS DETERMINED BY THE DIRECTOR, PREFERENCE FOR OFF-SITE REPLACEMENT SHALL BE ON PUBLIC PROPERTY.

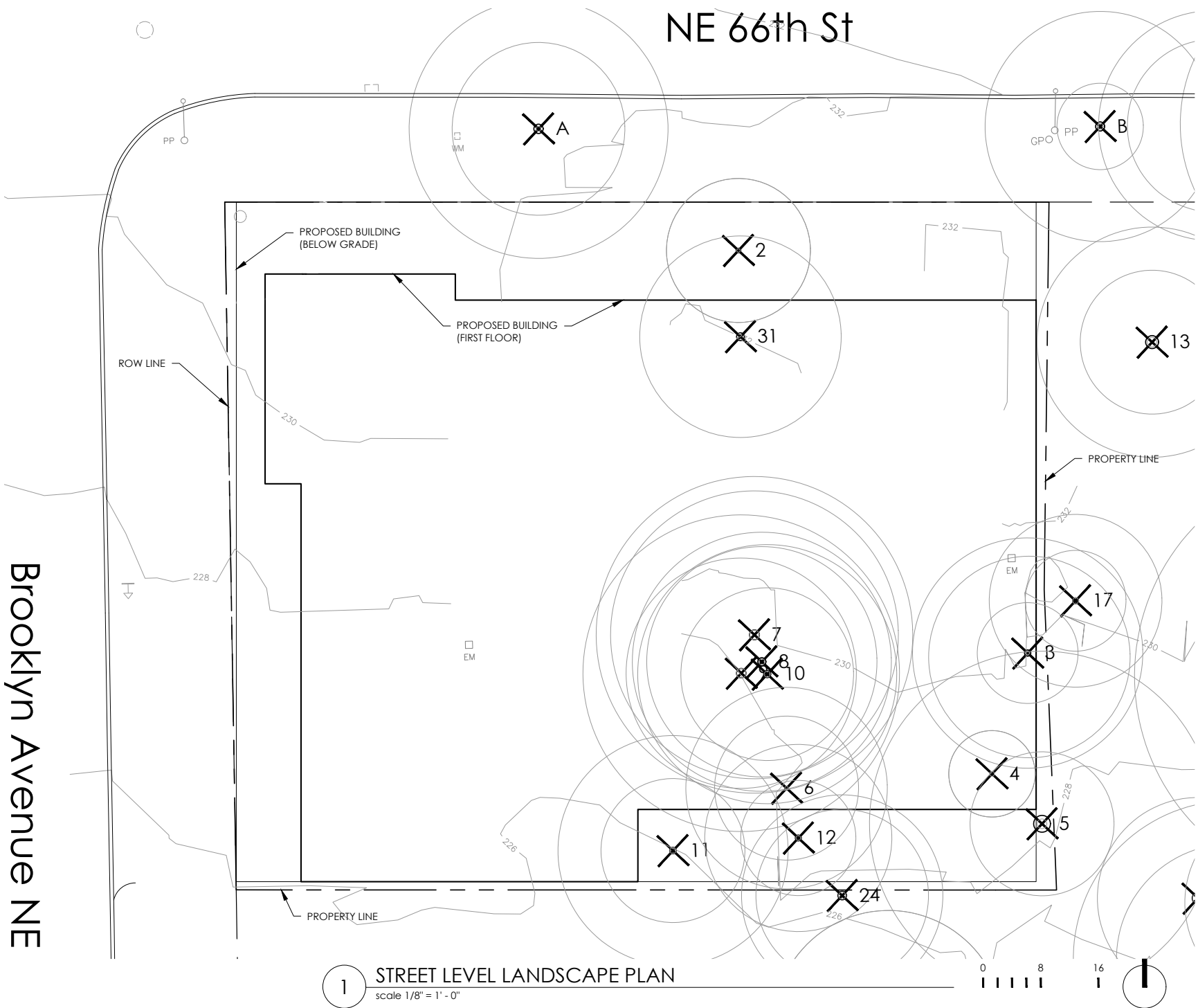
NO TREE REPLACEMENT IS REQUIRED IF THE (1) TREE IS HAZARDOUS, DEAD, DISEASED, INJURED OR IN A DECLINING CONDITION WITH NO REASONABLE ASSURANCE OF REGAINING VIGOR AS DETERMINED BY A TREE CARE PROFESSIONAL, OR (2) THE TREE IS PROPOSED TO BE RELOCATED TO ANOTHER SUITABLE PLANTING SITE AS APPROVED BY THE DIRECTOR.
2. EXISTING TREES PER ARBORIST, SEE REPORT FOR ADDITIONAL INFORMATION.

EXISTING TREES OVER 24" DBH PROPOSED FOR REMOVAL:		CANOPY AREA	
• TREE #5	28.1"	Thuja plicata	24'Ø Canopy AREA = 452 SF
			TOTAL CANOPY TO BE REPLACED = 452 SF

ONSITE REPLACEMENT TREES, SEE L1.01 - STREET LEVEL LANDSCAPE PLAN (CANOPY WIDTH PER SEATTLE GREEN FACTOR TREE LIST)

• (1) ACER CIRCINATUM / VINE MAPLE: MATURE CANOPY = 25'Ø = 1 X 490 SF EA.

TOTAL REPLACEMENT CANOPY: 490 SF





CODE:

Driveway Slope (SMC 23.54.030.D.3):

The Code requires that no portion of a driveway, whether located on a lot or on a right-of-way, shall exceed a slope of 15 percent, except as provided in this subsection 23.54.030.D.3. The maximum 15 percent slope shall apply in relation to both the current grade of the right-of-way to which the driveway connects, and to the proposed finished grade of the right-of-way if it is different from the current grade. The ends of a driveway shall be adjusted to accommodate an appropriate crest and sag. The Director may permit a driveway slope of more than 15 percent if it is found that:

- a. The topography or other special characteristic of the lot makes a 15 percent maximum driveway slope infeasible;
- b. The additional amount of slope permitted is the least amount necessary to accommodate the conditions of the lot; and
- c. The driveway is still useable as access to the lot.

REQUESTED RULING:

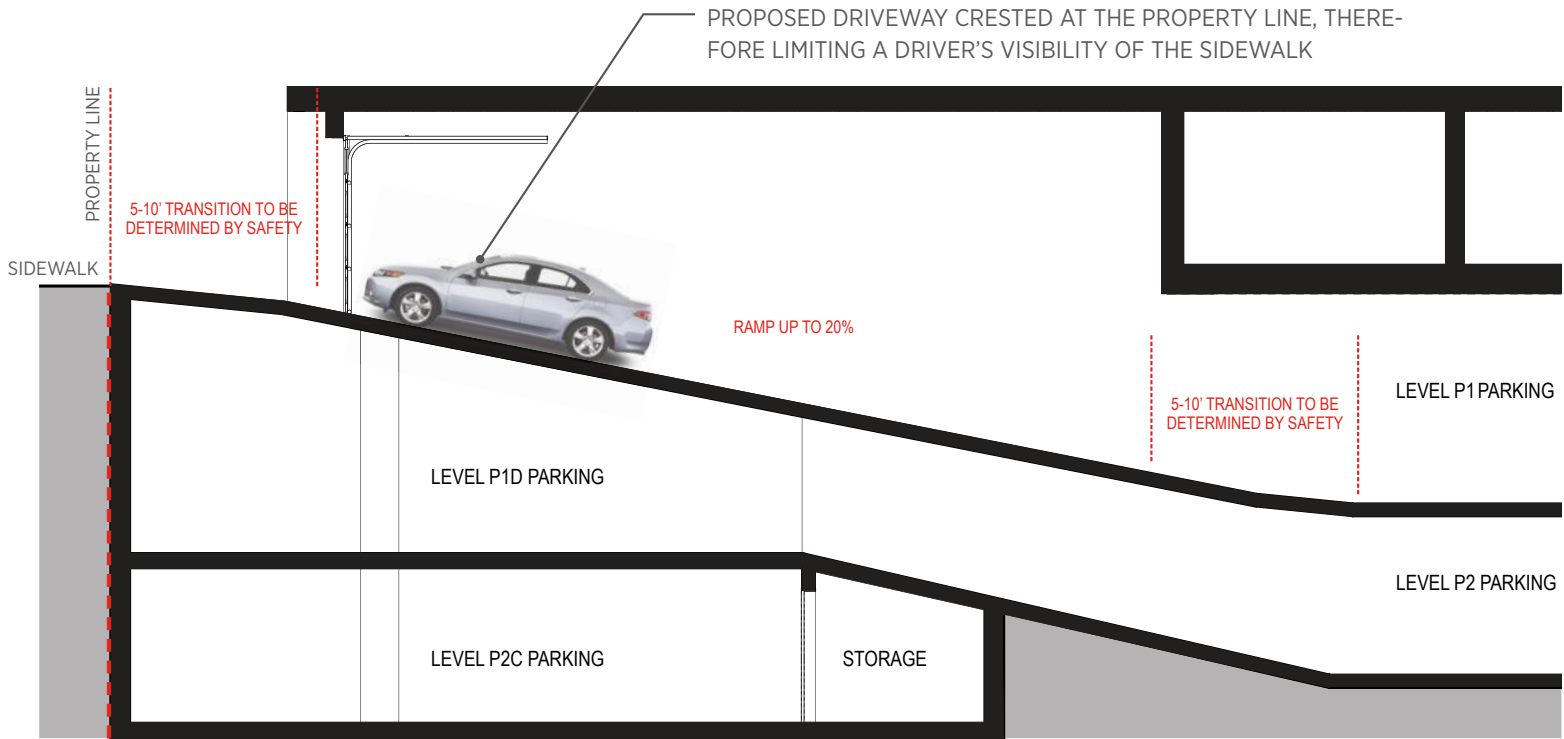
Permit a driveway slope of up a maximum of 25%.

RATIONALE:

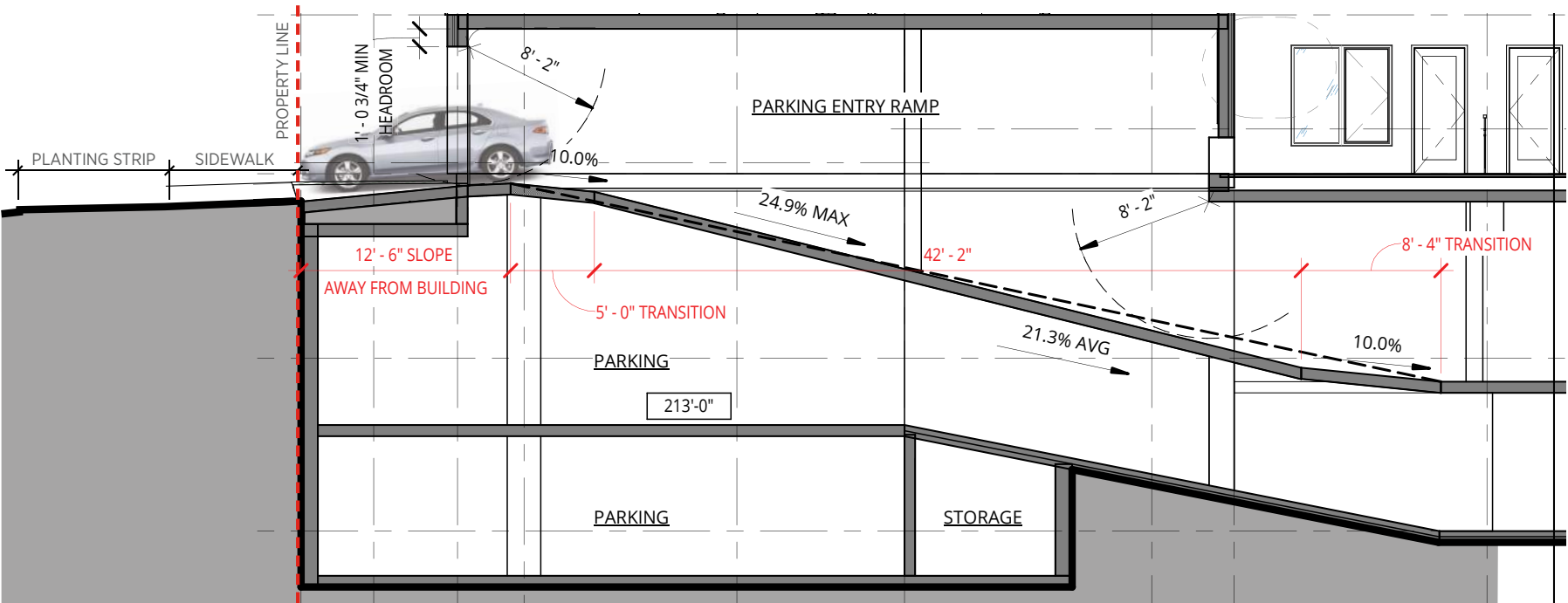
The Roosevelt Commercial Core and Pedestrian Overlay area is transitioning away from providing automobile parking. The project seeks to minimize the presence of covered at grade covered parking by placing all project parking below grade. In order to do this, the driveway ramp will need to increase to an average slope of 21.3% with a maximum slope of 24.9%. This is a product of required clearances and dimension available within the constraints of the small lot while providing a safe automobile approach at the street level that drains away from the building.

BOARD COMMENT AT EDG:

The Board indicated preliminary support of the Director approving the decision to approve this departure request although some Board members concerns about safety at the top of the ramp and wanted to see better documentation as to how and why the departure works.



DRIVEWAY SECTION AT EDG



PROPOSED DRIVEWAY SECTION

Proposed ramp slope is increased from “up to 20%” in the EDG proposal with a transition crest at the sidewalk grade to an average of 21.3% with a flat approach at grade between the sidewalk and garage door to increase pedestrian safety.



